

### Lead Agency Information

|                               |   |                                |          |
|-------------------------------|---|--------------------------------|----------|
| <b>Lead Agency Name:</b>      | County of Nevada Transit Services Division    |                                |          |
| <b>Address:</b>               | 950 Maidu Ave, Suite 170                      |                                |          |
| <b>City, State, Zip Code:</b> | Nevada City, CA 95949                         |                                |          |
| <b>County:</b>                | Nevada County                                 |                                |          |
| <b>Regional Entity:</b>       | Nevada County Local Transportation Commission |                                |          |
| <b>Title VI Attached:</b>     | Yes   | <b>Title VI Approval Date:</b> | 06/09/22 |

| Allocation Request Prepared by |  |
|--------------------------------|--|
| <b>Name:</b>                   | Robin Van Valkenburgh  |
| <b>Title:</b>                  | Transit Services Manager   |
| <b>Phone #:</b>                | 530-470-2833   |
| <b>E-mail:</b>                 | <a href="mailto:robin.vanvalkenburgh@nevadacour">robin.vanvalkenburgh@nevadacour</a> |

| Contact (if different than "Prepared by") |  |
|---|--|
| <b>Name:</b>                              |  |
| <b>Title:</b>                             |  |
| <b>Phone #:</b>                           |  |
| <b>E-mail:</b>                            |  |

| Authorized Agent |  |
|------------------|--|
| <b>Name:</b>     | Robin Van Valkenburgh/Heba El-Guindy   |
| <b>Title:</b>    | Transit Services Manager/Dir. Public Work  |
| <b>Phone #:</b>  | 530-470-2833/530-265-1721  |
| <b>E-mail:</b>   | <a href="mailto:heba.el-guindy@nevadacountyca.g">heba.el-guindy@nevadacountyca.g</a> |

| Legislative District Numbers |   |  |  |  |  |  |
|------------------------------|---|--|--|--|--|--|
| <b>Assembly*:</b>            | 1 |  |  |  |  |  |
| <b>Senate*:</b>              | 4 |  |  |  |  |  |
| <b>Congressional*:</b>       | 1 |  |  |  |  |  |

\*if you have additional Districts, please provide a separate attachment

### Project Summary

|   |  |   |  |
|---|--|---|--|
| <b>Name:</b> <i>No more than 180 characters.</i>                | Nevada County Connects Low-Income Pass Fare Subsidy Program  |   |  |
| <b>Description (Short):</b> <i>No more than 375 characters.</i> | Fare subsidy program designed to support and enhance mobility options for low-income persons, as defined by the California Department of Housing and Community Development, within the Nevada County Connects service area. The subsidy would be offered to persons who are referred through a partner agency and who have an annual income of 138-200 % of FPL. |   |  |
| <b>SB-942</b>   | No   | Agencies that elect to use SB 942 are required to use their apportionment concurrent to the initial submittal of their SB-942 project. <b>Agencies are limited to three years maximum to expend project funds using SB-942.</b> Missed submission of LCTOP Project Activity Reports (PAR) will result in a request to return funds to the LCTOP program. Agencies are required to submit annual expenditure reports along with their project activity reports. Agencies are not permitted to request a CAP for a transfer of funds to or from SB-942 projects. Cost savings and interest must be applied to the awarded project or returned to the LCTOP program. |  |
| <b>Type:</b>  | Operations   |   |  |
| <b>Sub-Type</b>   | Implementation of free or reduced fares  |   |  |

|                                  |          |                                     |            |
|----------------------------------|----------|-------------------------------------|------------|
| <b>Total Years of Rollover:</b>  | 0        | <b>Remaining Years of Rollover:</b> | 0          |
| <b>Start date (anticipated):</b> | 7/1/2023 | <b>End date (anticipated):</b>      | 06/01/2025 |

Please provide specific area information for the project in the Lat-Long tab.

|  |               |           |                            |         |                         |
|--|---------------|-----------|----------------------------|---------|-------------------------|
| <b>Project Life:</b> For capital projects, state the project useful life in years. For operation projects state the number of months a service will be funded. |               |           |                            |         |                         |
| <b>Capital:</b>  |               |           | <b>Operations:</b>         | 24      |                         |
| <b>Funding:</b>  | <b>99313:</b> | \$132,517 | <b>99314:</b>              | \$6,232 | <b>Total:</b> \$138,749 |
| <b>Approved LONP:</b>  |               |           | <b>LONP Approval date:</b> |         |                         |

LCTOP FY 2022-2023  
Allocation Request

**Funding Information**

| <i>LCTOP Allocation Year</i> | <b>Prior</b> | <b>FY 22-23</b> | <b>FY 23-24</b> | <b>FY 24-25</b> | <b>FY 25-26</b> | <b>Total</b> |
|------------------------------|--------------|-----------------|-----------------|-----------------|-----------------|--------------|
| <b>PUC 99313 Amount:</b>     |              | \$132,517       |                 |                 |                 | \$132,517    |
| <b>PUC 99314 Amount:</b>     |              | \$6,232         |                 |                 |                 | \$6,232      |
| <b>Total LCTOP Funds:</b>    | \$0          | \$138,749       | \$0             | \$0             | \$0             | \$138,749    |
| <b>Other GGRF:</b>           |              |                 |                 |                 |                 | \$0          |
| <b>Other Funds:</b>          |              |                 |                 |                 |                 | \$0          |
| <b>Total Project Cost:</b>   | \$0          | \$138,749       | \$0             | \$0             | \$0             | \$138,749    |

|                         |  |                |                        |
|-------------------------|--|----------------|------------------------|
| <b>Lead Agency:</b>     | County of Nevada Transit Services Division | <b>Amount:</b> | <b>PUC Funds Type:</b> |
| <b>Contact Person:</b>  | Robin Van Valkenburgh                      |                | 99313                  |
| <b>Contact Phone #:</b> | 530-470-2833                               | \$6,232        | 99314                  |
| <b>Contact E-mail:</b>  | robin.vanvalkenburgh@nevadacountyca.gov    |                |                        |

|                              |  |                |                        |
|------------------------------|--|----------------|------------------------|
| <b>Contributing Sponsor:</b> | Nevada County Transportation Commission                  | <b>Amount:</b> | <b>PUC Funds Type:</b> |
| <b>Contact Person:</b>       | Mike Woodman   | \$132,517      | 99313                  |
| <b>Contact Phone #:</b>      | 530-265-3202   |                | 99314                  |
| <b>Contact E-mails:</b>      | <a href="mailto:mwoodman@nccn.net">mwoodman@nccn.net</a> |                |                        |

|                              |  |                |                        |
|------------------------------|--|----------------|------------------------|
| <b>Contributing Sponsor:</b> |  | <b>Amount:</b> | <b>PUC Funds Type:</b> |
| <b>Contact Person:</b>       |  |                | 99313                  |
| <b>Contact Phone #:</b>      |  |                | 99314                  |
| <b>Contact E-mails:</b>      |  |                |                        |

|                              |  |                |                        |
|------------------------------|--|----------------|------------------------|
| <b>Contributing Sponsor:</b> |  | <b>Amount:</b> | <b>PUC Funds Type:</b> |
| <b>Contact Person:</b>       |  |                | 99313                  |
| <b>Contact Phone #:</b>      |  |                | 99314                  |
| <b>Contact E-mails:</b>      |  |                |                        |

|                              |  |                |                        |
|------------------------------|--|----------------|------------------------|
| <b>Contributing Sponsor:</b> |  | <b>Amount:</b> | <b>PUC Funds Type:</b> |
| <b>Contact Person:</b>       |  |                | 99313                  |
| <b>Contact Phone #:</b>      |  |                | 99314                  |
| <b>Contact E-mails:</b>      |  |                |                        |

**Total FY 22-23 LCTOP Funding** **\$138,749**

**Fully Funded Project:** Provide a description of all the funds that will be used to complete this project and how LCTOP funds will not supplant other funding sources. Include the project ID and awarded funding amount from prior rollover years.

This project is fully funded via the requested funding. No other funding is attributable to this program.

**Project Changes:** This section should be completed to detail any funds included in the "Prior" column of the Funding Information section above. For projects with an approved CAP that transferred funds and/or interest into the project from previous years, include the Project ID, amount of funds transferred, and CAP approval date.

N/A

LCTOP FY 2022-2023  
Allocation Request

| <b>Funding Plan</b>                |              |                 |                 |                 |                 |                 |                 |              |
|------------------------------------|--------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|--------------|
| <b>Proposed Total Project Cost</b> |              |                 |                 |                 |                 |                 |                 |              |
| <b>Component</b>                   | <b>Prior</b> | <b>FY 22-23</b> | <b>FY 23-24</b> | <b>FY 24-25</b> | <b>FY 25-26</b> | <b>FY 26-27</b> | <b>FY 27-28</b> | <b>Total</b> |
| PA&ED                              | \$0          | \$0             | \$0             | \$0             | \$0             | \$0             | \$0             | \$0          |
| PS&E                               | \$0          | \$0             | \$0             | \$0             | \$0             | \$0             | \$0             | \$0          |
| R/W                                | \$0          | \$0             | \$0             | \$0             | \$0             | \$0             | \$0             | \$0          |
| CON                                | \$0          | \$0             | \$0             | \$0             | \$0             | \$0             | \$0             | \$0          |
| Veh/Equip Purchase                 | \$0          | \$0             | \$0             | \$0             | \$0             | \$0             | \$0             | \$0          |
| Operations/Other                   | \$0          | \$0             | \$49,553        | \$49,553        | \$39,643        | \$0             | \$0             | \$138,749    |
| <b>TOTAL</b>                       | \$0          | \$0             | \$49,553        | \$49,553        | \$39,643        | \$0             | \$0             | \$138,749    |

| <b>Low Carbon Transit Operations Program (LCTOP)</b> |              |                 |                 |                 |                 |                 |                 |              |
|--|--------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|--------------|
| <b>Component</b>                                     | <b>Prior</b> | <b>FY 22-23</b> | <b>FY 23-24</b> | <b>FY 24-25</b> | <b>FY 25-26</b> | <b>FY 26-27</b> | <b>FY 27-28</b> | <b>Total</b> |
| PA&ED  |              |                 |                 |                 |                 |                 |                 | \$0          |
| PS&E   |              |                 |                 |                 |                 |                 |                 | \$0          |
| R/W  |              |                 |                 |                 |                 |                 |                 | \$0          |
| CON  |              |                 |                 |                 |                 |                 |                 | \$0          |
| Veh/Equip Purchase                                   |              |                 |                 |                 |                 |                 |                 | \$0          |
| Operations/Other                                     |              |                 | \$49,553        | \$49,553        | \$39,643        |                 |                 | \$138,749    |
| <b>TOTAL</b>   | \$0          | \$0             | \$49,553        | \$49,553        | \$39,643        | \$0             | \$0             | \$138,749    |

| <b>Funding Source:</b> |              |                 |                 |                 |                 |                 |                 |              |
|------------------------|--------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|--------------|
| <b>Component</b>       | <b>Prior</b> | <b>FY 22-23</b> | <b>FY 23-24</b> | <b>FY 24-25</b> | <b>FY 25-26</b> | <b>FY 26-27</b> | <b>FY 27-28</b> | <b>Total</b> |
| PA&ED                  |              |                 |                 |                 |                 |                 |                 | \$0          |
| PS&E                   |              |                 |                 |                 |                 |                 |                 | \$0          |
| R/W                    |              |                 |                 |                 |                 |                 |                 | \$0          |
| CON                    |              |                 |                 |                 |                 |                 |                 | \$0          |
| Veh/Equip Purchase     |              |                 |                 |                 |                 |                 |                 | \$0          |
| Operations/Other       |              |                 |                 |                 |                 |                 |                 | \$0          |
| <b>TOTAL</b>           | \$0          | \$0             | \$0             | \$0             | \$0             | \$0             | \$0             | \$0          |

| <b>Funding Source:</b> |              |                 |                 |                 |                 |                 |                 |              |
|------------------------|--------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|--------------|
| <b>Component</b>       | <b>Prior</b> | <b>FY 22-23</b> | <b>FY 23-24</b> | <b>FY 24-25</b> | <b>FY 25-26</b> | <b>FY 26-27</b> | <b>FY 27-28</b> | <b>Total</b> |
| PA&ED                  |              |                 |                 |                 |                 |                 |                 | \$0          |
| PS&E                   |              |                 |                 |                 |                 |                 |                 | \$0          |
| R/W                    |              |                 |                 |                 |                 |                 |                 | \$0          |
| CON                    |              |                 |                 |                 |                 |                 |                 | \$0          |
| Veh/Equip Purchase     |              |                 |                 |                 |                 |                 |                 | \$0          |
| Operations/Other       |              |                 |                 |                 |                 |                 |                 | \$0          |
| <b>TOTAL</b>           | \$0          | \$0             | \$0             | \$0             | \$0             | \$0             | \$0             | \$0          |

| <b>Funding Source:</b> |              |                 |                 |                 |                 |                 |                 |              |
|------------------------|--------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|--------------|
| <b>Component</b>       | <b>Prior</b> | <b>FY 22-23</b> | <b>FY 23-24</b> | <b>FY 24-25</b> | <b>FY 25-26</b> | <b>FY 26-27</b> | <b>FY 27-28</b> | <b>Total</b> |
| PA&ED                  |              |                 |                 |                 |                 |                 |                 | \$0          |
| PS&E                   |              |                 |                 |                 |                 |                 |                 | \$0          |
| R/W                    |              |                 |                 |                 |                 |                 |                 | \$0          |
| CON                    |              |                 |                 |                 |                 |                 |                 | \$0          |
| Veh/Equip Purchase     |              |                 |                 |                 |                 |                 |                 | \$0          |
| Operations/Other       |              |                 |                 |                 |                 |                 |                 | \$0          |
| <b>TOTAL</b>           | \$0          | \$0             | \$0             | \$0             | \$0             | \$0             | \$0             | \$0          |

### Funding Plan

| Funding Source:    |       |          |          |          |          |          |          |       |
|--------------------|-------|----------|----------|----------|----------|----------|----------|-------|
| Component          | Prior | FY 22-23 | FY 23-24 | FY 24-25 | FY 25-26 | FY 26-27 | FY 27-28 | Total |
| PA&ED              |       |          |          |          |          |          |          | \$0   |
| PS&E               |       |          |          |          |          |          |          | \$0   |
| R/W                |       |          |          |          |          |          |          | \$0   |
| CON                |       |          |          |          |          |          |          | \$0   |
| Veh/Equip Purchase |       |          |          |          |          |          |          | \$0   |
| Operations/Other   |       |          |          |          |          |          |          | \$0   |
| <b>TOTAL</b>       | \$0   | \$0      | \$0      | \$0      | \$0      | \$0      | \$0      | \$0   |

| Funding Source:    |       |          |          |          |          |          |          |       |
|--------------------|-------|----------|----------|----------|----------|----------|----------|-------|
| Component          | Prior | FY 22-23 | FY 23-24 | FY 24-25 | FY 25-26 | FY 26-27 | FY 27-28 | Total |
| PA&ED              |       |          |          |          |          |          |          | \$0   |
| PS&E               |       |          |          |          |          |          |          | \$0   |
| R/W                |       |          |          |          |          |          |          | \$0   |
| CON                |       |          |          |          |          |          |          | \$0   |
| Veh/Equip Purchase |       |          |          |          |          |          |          | \$0   |
| Operations/Other   |       |          |          |          |          |          |          | \$0   |
| <b>TOTAL</b>       | \$0   | \$0      | \$0      | \$0      | \$0      | \$0      | \$0      | \$0   |

| Funding Source:    |       |          |          |          |          |          |          |       |
|--------------------|-------|----------|----------|----------|----------|----------|----------|-------|
| Component          | Prior | FY 22-23 | FY 23-24 | FY 24-25 | FY 25-26 | FY 26-27 | FY 27-28 | Total |
| PA&ED              |       |          |          |          |          |          |          | \$0   |
| PS&E               |       |          |          |          |          |          |          | \$0   |
| R/W                |       |          |          |          |          |          |          | \$0   |
| CON                |       |          |          |          |          |          |          | \$0   |
| Veh/Equip Purchase |       |          |          |          |          |          |          | \$0   |
| Operations/Other   |       |          |          |          |          |          |          | \$0   |
| <b>TOTAL</b>       | \$0   | \$0      | \$0      | \$0      | \$0      | \$0      | \$0      | \$0   |

| Funding Source:    |       |          |          |          |          |          |          |       |
|--------------------|-------|----------|----------|----------|----------|----------|----------|-------|
| Component          | Prior | FY 22-23 | FY 23-24 | FY 24-25 | FY 25-26 | FY 26-27 | FY 27-28 | Total |
| PA&ED              |       |          |          |          |          |          |          | \$0   |
| PS&E               |       |          |          |          |          |          |          | \$0   |
| R/W                |       |          |          |          |          |          |          | \$0   |
| CON                |       |          |          |          |          |          |          | \$0   |
| Veh/Equip Purchase |       |          |          |          |          |          |          | \$0   |
| Operations/Other   |       |          |          |          |          |          |          | \$0   |
| <b>TOTAL</b>       | \$0   | \$0      | \$0      | \$0      | \$0      | \$0      | \$0      | \$0   |

| Funding Source:    |       |          |          |          |          |          |          |       |
|--------------------|-------|----------|----------|----------|----------|----------|----------|-------|
| Component          | Prior | FY 22-23 | FY 23-24 | FY 24-25 | FY 25-26 | FY 26-27 | FY 27-28 | Total |
| PA&ED              |       |          |          |          |          |          |          | \$0   |
| PS&E               |       |          |          |          |          |          |          | \$0   |
| R/W                |       |          |          |          |          |          |          | \$0   |
| CON                |       |          |          |          |          |          |          | \$0   |
| Veh/Equip Purchase |       |          |          |          |          |          |          | \$0   |
| Operations/Other   |       |          |          |          |          |          |          | \$0   |
| <b>TOTAL</b>       | \$0   | \$0      | \$0      | \$0      | \$0      | \$0      | \$0      | \$0   |

## **Project Information**

**1) Project Description** - Provide a comprehensive project description. For operations projects, include: [number of trips, span, frequency improvements, number of days of operation and marketing component \(if applicable\)](#). For capital projects, include: [product specifications](#) and identify [all LCTOP funded components](#). **No more than 12 lines.**

This project will continue a previously funded low-income pass subsidy program. The program is designed to increase accessibility of transit to low-income community members by providing subsidized Nevada County Connects fixed route bus passes to eligible persons. Eligibility is based on an annual income of 138 - 200 percent of the Federal Poverty Level, and the consumer may not currently receive transit benefits via another state or federal program. All consumers are referred by partner agencies who provide income verification. The low-income pass subsidy program currently provides monthly bus passes to approximately 90 consumers per month, which in the FY21-22 year resulted in approximately 20,000 passenger boardings or 20 percent of total system ridership. Bus service is provided Mon - Fri 6:00 AM- 8:00 PM and Sat 7:30 AM - 5:00 PM. The project service area includes all bus routes serving Western Nevada County; Grass Valley, Nevada City, Penn Valley, Alta Sierra, North San Juan, Lake of the Pines with regional connections at the Auburn Amtrak station.

**2) Project Planning** - Provide a detailed explanation of the project planning process and how it was designed to avoid substantial burden on any low- income, disadvantaged, and vulnerable populations. [Include any public outreach efforts, engagement events, community input, and workshops](#). **No more than 10 lines.**

This is the continuation of our current Low-Income Pass Subsidy program, which has proven to be very successful in meeting an unmet need within the community. This program was developed through discussions and outreach with local partner agencies, including: FREED Center for Independent Living, the Aging and Disability Resource Council, County Behavioral Health, Adult Protective Services and Department of Social Services, the Homeless Outreach Team and the Social Services Transportation Advisory Committee. The project is supported by recommendations within the most recent Transportation Development Plan and Coordinated Public Transit - Human Services Transportation Plan updates completed in June 2021.

**3) Project Costs** - Provide an [itemized breakdown](#) of project components and describe [how the cost estimations were developed](#). Please include marketing and bus wraps cost in this section. **Total costs must correspond to the Funding Information section above. No more than 12 lines.**

The project addresses two fare sets: the zone 1 (Grass Valley & Nevada City local service) at \$45.00 per pass, and zone 2 regional (Alta Sierra, Penn Valley, North San Juan, Lake of the Pines and Auburn) at \$90.00 per pass. The current program provides approximately 1,080 passes per year which equate to approximately 18,000 - 20,000 passenger trips per year. This is approximately 20 percent of total annual ridership. It is difficult to identify the true impact of the passes as we cannot track specific users, but this program is supporting an increase in overall ridership of approximately 10 percent from FY21-22 to FY 22-23.

**Agency Information**

**4) Agency Fare** - Describe the fare structure for your system and how the project will affect that structure if at all.

The Nevada County Connects fare structure is a two zone system: zone 1 local (Grass Valley and Nevada City) at \$1.50 single ride, \$0.75 Discount (senior, youth, disabled), monthly passes are \$45.00 adult and \$22.50 Discount; zone 2 regional (Alta Sierra, Penn Valley, Lake of the Pines, North San Juan and Auburn) \$3.00 adult single ride, \$1.50 Discount, monthly passes are \$90.00 adult and \$45.00 Discount. This project will not effect fares.

**5) Agency Service** - Describe the transit service provided and how the project plays into the overall operations. Include how the COVID-19 pandemic has impacted transit service related to the project. *No more than 10 lines.*

The project will operate within the current Nevada County Connects operating days and hours; Monday-Friday 6:00am-8:00pm & Saturday 7:30am-5:00pm and serve our defined service area. The fixed route bus system operates throughout Western Nevada County including; Nevada City, Grass Valley, Penn Valley, Lake Wildwood, Alta Sierra, Lake of the Pines and a regional connector to Auburn Amtrak. The proposed project meets recommendations from local planning documents and does not alter services provided. Public outreach and marketing will educate the community about the program, who is eligible and how to access the bus. The fare subsidy program will provide subsidized monthly bus passes to eligible persons, thereby increasing ridership and public awareness of the transit system. This project addresses identified unmet needs within the community due to the fact that low-income is currently not a qualifying factor in receiving discounted fares.

## Project GHG Benefits

|  |                |  |
|--|----------------|--|
| <b>Greenhouse Gas Reductions - <u>Describe qualitatively</u> how this project will reduce greenhouse gas emissions.</b>  |                |  |
| The project will improve access to our rural fixed route public transit by offering a mobility option for low-income persons that connects to employment, education and community events. The current program provides approximately 18,000 trips annually, thereby reducing VMT and greenhouse gas emissions. The project includes collaboration with organizations throughout the community to assist in outreach efforts and supports the education of, and sharing information on, the benefits of the program as they relate to using public transportation and reducing emissions. |                |  |
| <b>Greenhouse Gas Reductions - Please provide quantitative information requested below and explanations supporting the data provided.</b>  |                |  |
|  | <b>Value</b>   | <b>Explanation</b>   |
| <b>Year 1 (Yr1)</b> - <i>First year of service, or year that capital improvements will be completed.</i>   | 2023           | The first year of service to be funded through this LCTOP allocation request   |
| <b>Year F (YrF)</b> - <i>Final year that the service is funded or the final year of useful life for capital improvements.</i>  | 2026           | The last year of service expected to be funded through this LCTOP allocation request.  |
| <b>Project Yr. 1 Ridership Increase</b> - <i>Estimated annual ridership contributed by the new service or capital improvement in Yr1. Refer to page 5 of the Supplemental Guidance.</i>  | 17,928         | Total estimated ridership based upon current program use providing 1,080 monthly passes annually (90 passes/month) x 16.6 passenger trips/pass/month.                            |
| <b>Project Yr. F Ridership Increase</b> - <i>Estimated annual ridership contributed by the new service or capital improvement in YrF. Refer to page 5 of the Supplemental Guidance.</i>  | 18,900         | Total estimated ridership based upon providing 1,080 monthly passes (90 passes per month) x 17.5 passenger trips/pass/month.   |
| <b>Adjustment (A)</b> - <i>Adjustment factor to account for Choice Riders. Use default values listed on page 5 of the Supplemental Guidance.</i>   | 0.670          | The Nevada County Connects fixed route transit service provides both local and long distance communte services, therefore an average of the two suggested defaults was utilized. |
| <b>Trip Length (L)</b> - <i>Length (miles) of average auto trip reduced or average passenger trip length. You may use default values list ed on page 17 of the Supplemental Guidance.</i>  | 3.24           | Average trip length was obtained by dividing FY21-22 annual service miles (322,267) by total boardings (99,321).   |
| <b>Project Useful Life</b>   | <b>3</b>       | This is calculated based on the values above.  |
| <b>Total Project Ridership Increased</b>   | <b>55,242</b>  | This is calculated based on the values above.  |
| <b>Total Project VMTs Reduced</b>  | <b>119,919</b> | This number is calculated based on the values above.   |
| <b>Total Project GHG Emission Reductions (MTCO<sub>2e</sub>)</b>   | <b>49.11</b>   | This number is calculated based on the values from above and the <u>QM-Tool tab</u> .  |
| <b>LCTOP Project GHG Emission Reductions (MTCO<sub>2e</sub>)</b>   | <b>49.11</b>   | This number is calculated based on the values from above and the <u>QM-Tool tab</u> .  |

## Project Benefits

**Job Support Benefits** *(Refer to LCTOP Guidelines and CARB Co-Benefits website for more information)*

|  |                   |
|--|-------------------|
| <b>Primary Project Activity</b> <i>(select from drop-down)</i> | Transit subsidies |
| <b>% of Project Budget Associated with Primary Activity</b>    | 100%              |
| <b>Other Project Activity</b> <i>(select from drop-down)</i>   |                   |
| <b>% of Project Budget Associated with Other Activity</b>      |                   |
| <b>Other Project Activity</b> <i>(select from drop-down)</i>   |                   |
| <b>% of Project Budget Associated with Other Activity</b>      |                   |

**Travel Cost Savings Benefits**

[Refer to page 6 on the Supplemental Guidance.](#)

|  | Value         | Explanation   |
|--|---------------|---|
| <b>Baseline Average One-Way Fare Cost (\$/One-Way Trip/Rider)</b> <i>(Average fare per way prior to project implementation)</i>  | <b>\$2.01</b> | This is the average fare for FY21-22 service year based on an adult single ride cash fare of \$1.50 zone 1 and \$3.00 zone 2. |
| <b>New Average One-Way Fare Cost (\$/One-Way Trip/Rider)</b> <i>(Average fare per way resulting from project implementation)</i> | <b>\$2.25</b> | This program provides full fare monthly passes at \$45.00 zone 1 and \$90.00 zone 2. The average cash fare would be \$2.25.   |
| <b>Transit Facility Parking Cost (\$/Roundtrip/Rider)</b> <i>(Average cost to park to use transit associated with project)</i>   | <b>\$0.00</b> | (ex. The average transit facility parking cost is \$5 per day)  |
| <b>Avoided Parking Cost (\$/Roundtrip/Rider)</b> <i>(Average avoided parking cost associated with project)</i>                   | <b>\$0.00</b> | (ex. The average parking cost in the project area is \$15 per day)  |
| <b>Avoided Toll Cost (\$/Roundtrip/Rider)</b> <i>(Average avoided toll cost associated with project)</i>                         | <b>\$0.00</b> | (ex. The average tolling cost in the project area is \$10 per day)  |

**Co-Benefits - Check all additional Benefits/Outcomes.**

- |   |  |
|---|--|
| <input checked="" type="checkbox"/> <b>Improved Safety</b><br><input checked="" type="checkbox"/> <b>Improved Public Health</b><br><input type="checkbox"/> <b>Reduced Operating/Maintenance Costs</b><br><input type="checkbox"/> <b>Increase System Reliability</b> | <input checked="" type="checkbox"/> <b>Coordination with Educational Institution</b><br><div style="margin-left: 20px;"><input checked="" type="checkbox"/> <b>College</b>    <input checked="" type="checkbox"/> <b>Grades K-12</b></div> <input checked="" type="checkbox"/> <b>Promotes Active Transportation</b><br><input checked="" type="checkbox"/> <b>Promotes Integration w/ other modes</b> |
|---|--|

**Co-Benefits - Describe benefits selected above and other benefits not listed.**

The proposed project supports improved safety by providing bus passes to low-income individuals who may otherwise walk along highways or hitch hike to get to their destinations. The project supports improved public health by providing access to mental and physical healthcare locations, employment and education opportunities, including local community college and K-12 schools, and recreational areas within the community such as the Yuba River. Since every bus is fitted with bike racks and Nevada County Transit policy allows for the use and transport of foldable electric scooters, this program may improve participation in active transportation, and offers significant opportunity to connect with regional services including light rail and Amtrak.



**Priority Populations Benefits**

|   |  |
|---|--|
| Does your Agency's Service Area have a Disadvantaged Community? (as defined by SB 535)  | No   |
| Is the project located within the boundaries of a disadvantaged community census tract?   | No   |
| Is the project located within the boundaries of a low-income community census tract?  | Yes  |
| Is the project located outside of a disadvantaged community, but within 1/2 mile of a disadvantage community and within a low-income census tract?  | No   |
| Amount of FY 22-23 funds benefitting Disadvantaged Communities : \$   | 0  |
| <b>Priority Population Community Engagement: Identify the specific assessment for the Community Engagement Co-benefit (High, Medium, Low):</b> <i>*See Page 30 of the Supplemental Guidance</i> | Low  |
| <u>Method:</u> Select the method your agency used for identifying an important community or household need.   | D. Where direct engagement is infeasible, make a selection below that best addresses the needs of the priority population for the project.   |
| <u>Specific Common Need:</u> Make a selection only if <a href="#">letter D</a> is selected above.   | ECON 5 Reduce transportation costs (e.g., free or reduced cost transit passes) and improve access to public transportation (e.g., new services in under-served urban and rural communities). |

**Priority Populations Community Needs Description:** Expound on the selections above in **Method** and **Specific Common Need** to describe the process that your agency used to identify important community needs. Provide details of any public outreach efforts, engagement events, community input, and workshops.

This project was developed based on strategy recommendations from the 2021 Nevada County Coordinated Public Transit - Human Services Transportation Plan and direct communications with agency partners. Public outreach was held throughout the plan development and was most recently offered when the project was proposed to the Transit Services Commission on March 16, 2022. Ongoing public outreach occurs through the ATCI-MAPCO/SSTAC quarterly meetings and the every other month Transit Services Commission meetings. Implementation of the proposed project will benefit all communities served by Nevada County Connects, a significant proportion of which are AB1550 designated low-income populations.

**Priority Populations Benefits**

|   |                                       |
|---|---------------------------------------|
| <b>Identify the Priority Population(s) that will benefit from this project.</b> | Project provides benefits to a LIC/HH |
|---|---------------------------------------|

|   |  |
|---|--|
| <b>Priority Population Benefit:</b> Select the benefit your project provides to the community or household. | F. Project improves combined housing and transportation affordability. |
|---|--|

**Priority Population Benefit:** Based on the selections above, explain in greater detail how the project will provide benefits to the priority populations in your service area.

By providing subsidized monthly bus passes to low-income individuals we are removing a cost barrier to access myriad of services such as: employment, education, health and mental health services and local and regional recreation activities, as well as local cultural and entertainment events.

**SB 1119 Project Criteria: See page 7 of the LCTOP Supplemental Guidance for more information.**

|  |     |
|--|-----|
| <b>Is the project a transit fare subsidies or network and fare integration technology improvements, including, but not limited to, discounted or free student transit passes</b> | Yes |
|--|-----|

|  |    |
|--|----|
| <b>Is the project a purchase of zero-emission transit buses and/or purchase and installation of supporting infrastructure?</b> | No |
|--|----|

|   |    |
|---|----|
| <b>Is the project a new or expanded transit service that connects with transit service serving a disadvantaged communities?</b> | No |
|---|----|

**SB 1119 Project Criteria:** If this is a new or expanded service project, explain how it connects to a transit service that serves a Disadvantaged Community.

N/A



**Note to applicants:**

A step-by-step user guide, including project examples, for this Benefits Calculator Tool is available at <https://ww2.arb.ca.gov/sites/default/files/classic/cc/capandtrade/auctionproceeds/caltrans>.

**Step 2a: Identify the Project Type.**

**Step 2b: Input Project-specific Information.**

|                      |   |
|----------------------|---|
| <b>Project Name:</b> | Nevada County Connects Low-Income Pass Fare Subsidy Project |
|----------------------|---|

This section is used to determine the quantification method and emission factors to use to

| Project Info Inputs   | Input                                   | Required       |
|-----------------------|---|----------------|
| Project Type          | Implementation of free or reduced fares | Required Input |
| Quantification Method | Increased Ridership                     | Automated      |
| Service Type          | Local/ Intercity Bus (Short Distances)  | Required Input |
| Type of Region        | County                                  | Required Input |
| Region                | Nevada                                  | Required Input |
| Year 1 (Yr1)          | 2023                                    | Required Input |
| Year F (YrF)          | 2026                                    | Required Input |
| Useful Life (yrs)     | 3                                       | Calculated     |

This section is used to estimate the emission and cost reductions from displaced auto vehicles

| Displaced Auto VMT Inputs                     | Input   | Required       |
|---|---------|----------------|
| Yr1 Ridership                                 | 17,928  | Required Input |
| YrF Ridership                                 | 18,900  | Required Input |
| Adjustment Factor                             | 0.6700  | Required Input |
| Length of Average Trip (mi)                   | 3.24    | Required Input |
| Passenger VMT Reductions (mi)                 | 119,919 | Calculated     |
| GHG Emission Reductions (MTCO <sub>2</sub> e) | 49      | Calculated     |

This section is used to estimate the net emission reductions from new service or from the

| New Service Vehicle Inputs | Input | Required     |
|----------------------------|-------|--------------|
| Vehicle Type               |       | Not Required |
| Engine Tier                |       | Not Required |

|  |       |                |
|--|-------|----------------|
| Engine Horsepower  |       | Not Required   |
| Fuel Type  |       | Not Required   |
| Hybrid Vehicle   |       | Not Required   |
| Model Year   |       | Not Required   |
| Project-Specific GHG Emission Factor (gCO <sub>2</sub> e/MJ)                               |       | Not Required   |
| Annual VMT (mi/yr)   |       | Not Required   |
| Annual Fuel Use  |       | Not Required   |
| Annual Renewable Energy Generated (kWh/yr)   |       | Not Required   |
| GHG Emissions (MTCO <sub>2</sub> e)  |       | Not Applicable |
| This section is used to estimate the net emission reductions from vehicle replacement as a |       |                |
| Baseline Vehicle Inputs  | Input | Required       |
| Vehicle Type   |       | Not Required   |
| Engine Tier  |       | Not Required   |
| Engine Horsepower  |       | Not Required   |
| Fuel Type  |       | Not Required   |
| Hybrid Vehicle   |       | Not Required   |
| Model Year   |       | Not Required   |
| Project-Specific GHG Emission Factor (gCO <sub>2</sub> e/MJ)                               |       | Not Required   |
| Annual VMT (mi/yr)   |       | Not Required   |
| Annual Fuel Use  |       | Not Required   |
| GHG Emission Reductions (MTCO <sub>2</sub> e)  |       | Not Applicable |

This section is used to estimate the net emission reductions from fuel/energy reductions as

| Fuel/Energy Reductions Inputs                 | Input | Required       |
|---|-------|----------------|
| Vehicle Type                                  |       | Optional Input |
| Engine Tier                                   |       | Not Required   |
| Engine Horsepower                             |       | Not Required   |
| Fuel Type                                     |       | Not Required   |
| Model Year                                    |       | Not Required   |
| Annual Fuel/Energy Reduced                    |       | Not Required   |
| GHG Emission Reductions (MTCO <sub>2</sub> e) |       | Calculated     |

This section is used to estimate the travel cost savings as a result of the proposed project.

| Travel Cost Savings Inputs                                 | Input  | Required       |
|--|--------|----------------|
| Baseline Average One-Way Fare Cost (\$/One-Way Trip/Rider) | \$2.01 | Required Input |
| New Average One-Way Fare Cost (\$/One-Way Trip/Rider)      | \$2.25 | Required Input |
| Average Transit Facility Parking Cost (\$/Roundtrip/Rider) | \$0.00 | Required Input |
| Average Avoided Parking Cost (\$/Roundtrip/Rider)          | \$0.00 | Required Input |
| Average Avoided Toll Cost (\$/Roundtrip/Rider)             | \$0.00 | Required Input |

This section is used to estimate the travel cost savings as a result of the proposed project.

|  |    |            |
|--|----|------------|
| Total Project GHG Emission Reductions (MTCO <sub>2</sub> e)            | 49 | Calculated |
| Total LCTOP Project GHG Emission Reductions (MTCO <sub>2</sub> e)      | 49 | Calculated |
| FY 2022-23 LCTOP Project GHG Emission Reductions (MTCO <sub>2</sub> e) | 49 | Calculated |

ces Board  
ool for the  
ations Program  
vestments

available here:  
[lctop\\_finaluserguide\\_20-21.pdf](#)

ogram

|  |
|--|
| estimate emissions.  |
| Description  |
| For the purposes of this quantification, eligible LCTOP projects fall into four project types. Select the project type that best describes this component.   |
| Emission Estimates = Emission Reductions from Displaced Autos  |
| The transit service (e.g., Intercity/Express Bus (Long Distance), Light Rail, Vanpool, etc.) directly associated with the proposed project. For projects that serve multiple services, select Multi-modal. |
| The type of region that best encompasses the geographic location for the proposed project type.  |
| The County or Air Basin where the majority of the service occurs.  |
| The first year of operation for the new expanded/enhanced service - funded by FY 2022-23 LCTOP funds.  |
| The final year of operation for the new expanded/enhanced service - funded by FY 2022-23 LCTOP funds.  |
| The number of years the service is funded by FY 2020-21 LCTOP funds. Limited to up to 50 years.  |
| icle miles traveled (VMT).   |
| Description  |
| The increase in unlinked passenger trips directly associated with the proposed project in the first year (Yr1).  |
| The increase in unlinked passenger trips directly associated with the proposed project in the final year. If the ridership is not expected to change, Yr1 and YrF should be the same value.                |
| Discount factor applied to annual ridership to account for transit-dependent riders. Use: Document project-specific data or system average developed from a recent, statistically valid survey or default. |
| Annual passenger miles over unlinked trips directly associated with the proposed project.  |
| The estimated displaced auto VMT from the proposed project.  |
| The estimated GHG emission reductions in metric tons (MT) of carbon dioxide equivalent (CO2e) from displaced auto VMT from the proposed project.   |
| urchase of new zero-emission/hybrid vehicle(s).  |
| Description  |
| Not applicable for this project type.  |
| Not applicable for this project type.  |

|  |
|--|
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for the selected fuel type. |
| Not applicable for this project type.      |
| a result of the proposed project.          |
| Description                                |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |
| Not applicable for this project type.      |

|   |
|---|
| s a result of the proposed project.   |
| Description   |
| The vehicle type (e.g., Transit Bus, Streetcar, Ferry, etc.) of the vehicle(s) that will realize fuel/energy reductions as a result of The project.   |
| Not applicable for this project type.   |
| Not applicable for this project type.   |
| Not applicable for this project type.   |
| Not applicable for this project type.   |
| Not applicable for this project type.   |
| Not applicable for this project type.   |
| Not applicable for this project type.   |
| Description   |
| The average fare cost per one-way trip per rider prior to project implementation. If expanding service, baseline fare cost is zero.   |
| The new expected average fare cost per one-way trip per rider resulting from the proposed project.  |
| The average expected cost of parking per roundtrip per rider that riders would pay at the transit facility where the trip originates. Consider that not all transit riders may use the parking. However, the calculations will already take into account that parking is only paid once per roundtrip, so do not divide this value by two to account for one-way trips. |
| The average expected cost of parking per roundtrip per rider that riders would have otherwise paid if not using the service resulting from the project. The calculations will already take into account that parking is only paid once per roundtrip, so do not divide this value by two to account for one-way trips.  |
| The average expected cost of tolls per roundtrip per rider that riders would have otherwise paid if not using the service resulting from the project. The calculations will already take into account that tolls are only paid once per roundtrip, so do not divide this value by two to account for one-way trips.   |
| Total GHG emission reductions (MTCO2e) from the project during the useful life.   |
| The portion of GHG emission reductions attributable to funding from LCTOP; GHG emission reductions are prorated according to the level of program funding contributed from LCTOP and other GGRF-funded programs, as applicable.   |
| The portion of GHG emission reductions attributable to funding from FY 20-21 LCTOP; GHG emission reductions are prorated according to the level of program funding contributed from FY 20-21 LCTOP and other GGRF-funded programs, as applicable.   |







Project Location Information

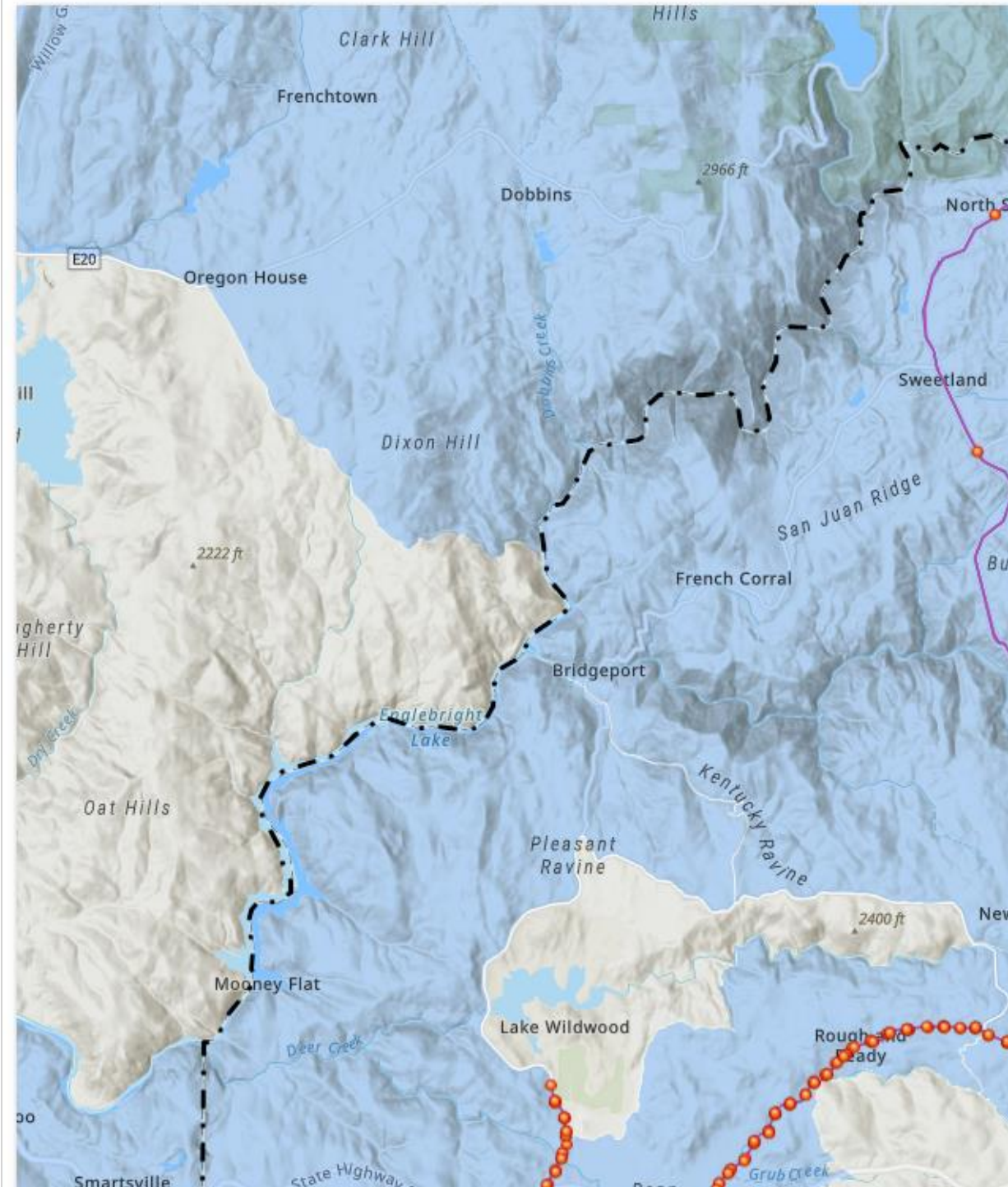
d

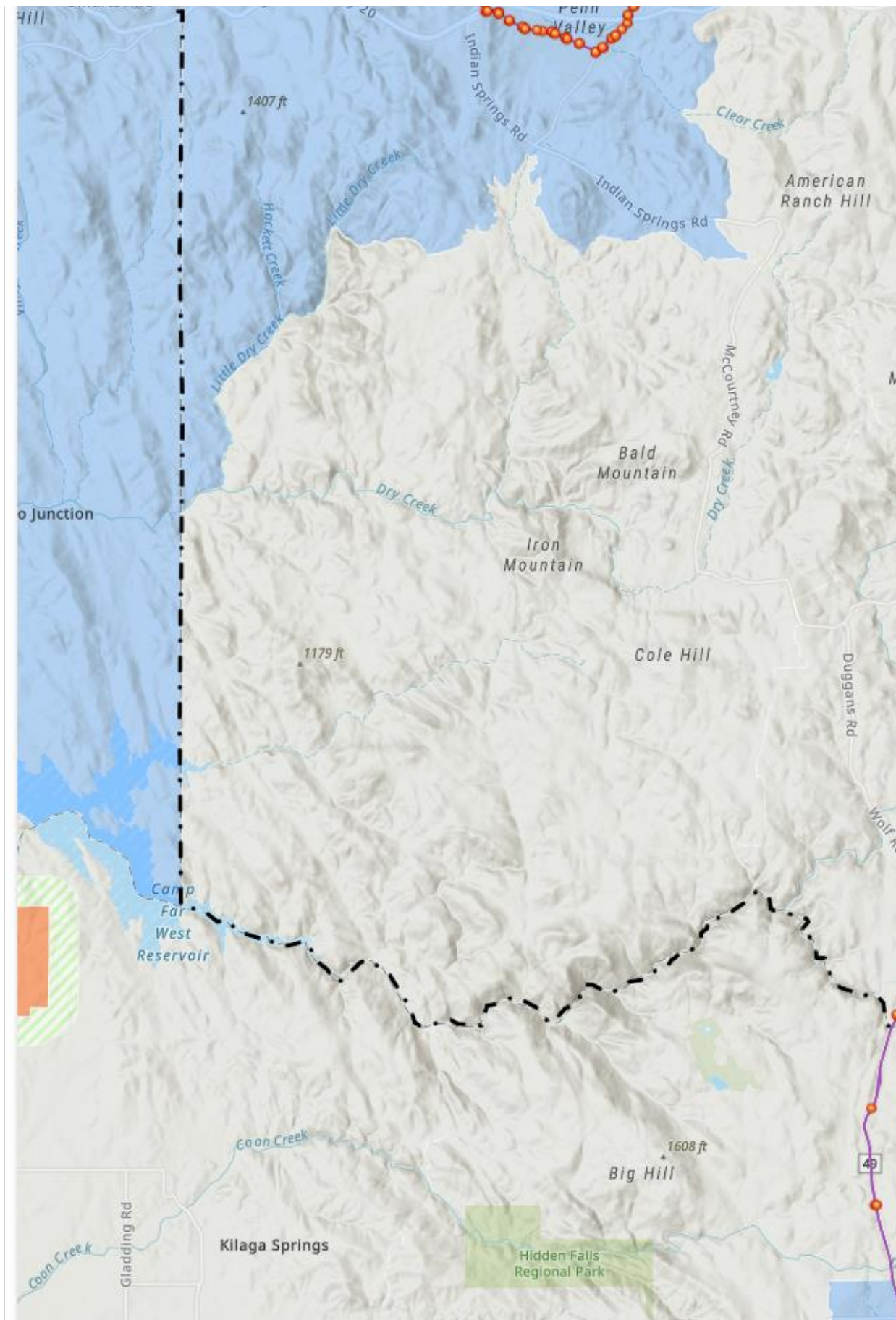
d  
d  
d  
d  
d  
d  
d  
d  
d  
d  
d  
d  
d

Please insert a screenshot of the [project area](https://webmaps.arb.ca.gov/PriorityPopulations/) from the CARB Greenhouse Gas Reduction Fund Proj  
<https://webmaps.arb.ca.gov/PriorityPopulations/>

Please insert a screenshot of the project area on the CARB Greenhouse Gas Reduction Fund Proj  
Map Use <https://oehha.ca.gov/calenviroscreen/report/calenviroscreen-40> to identify your DAC and  
<https://webmaps.arb.ca.gov/PriorityPopulations/> to Identify your AB 1550 Community

See attached map.

























































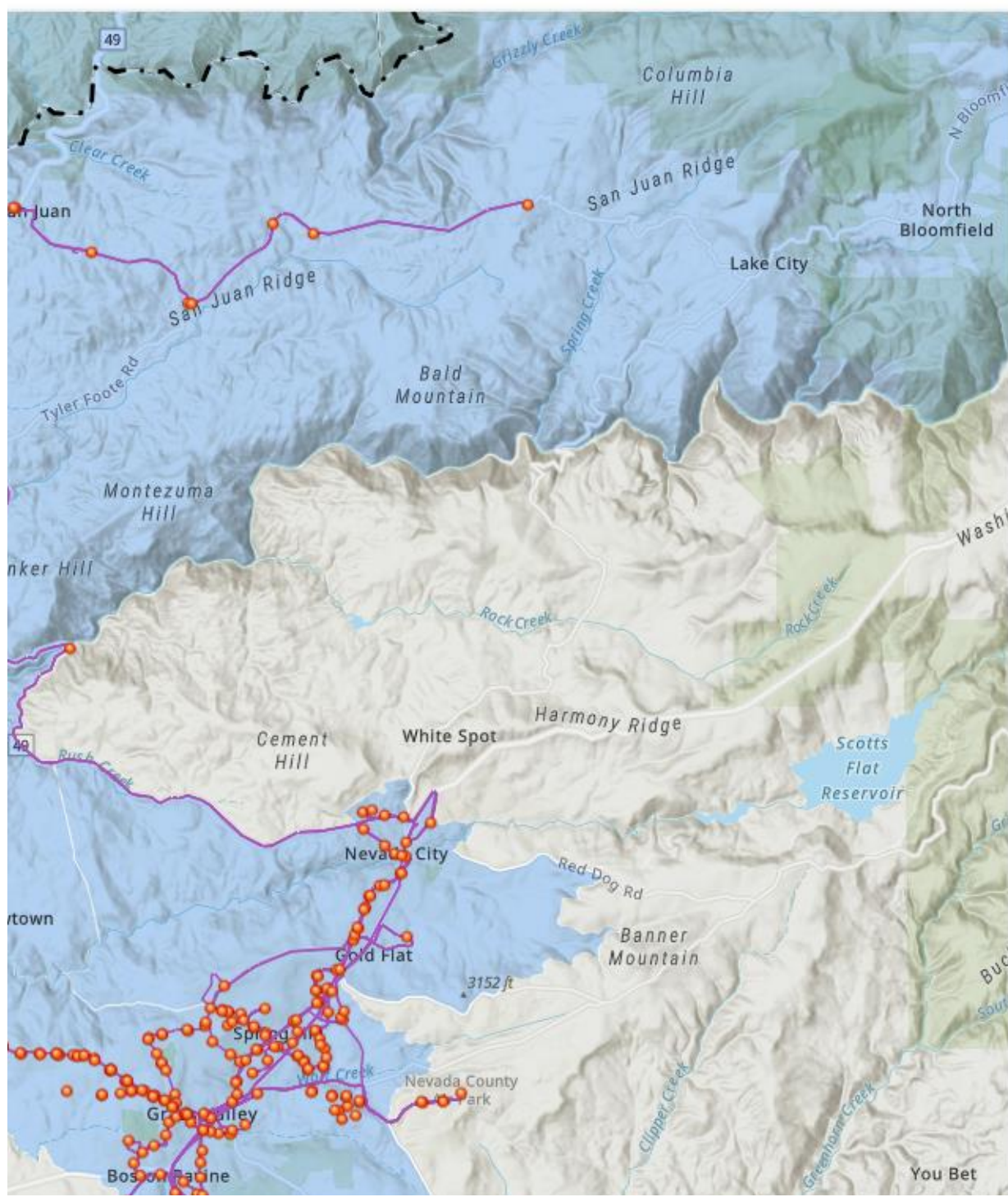




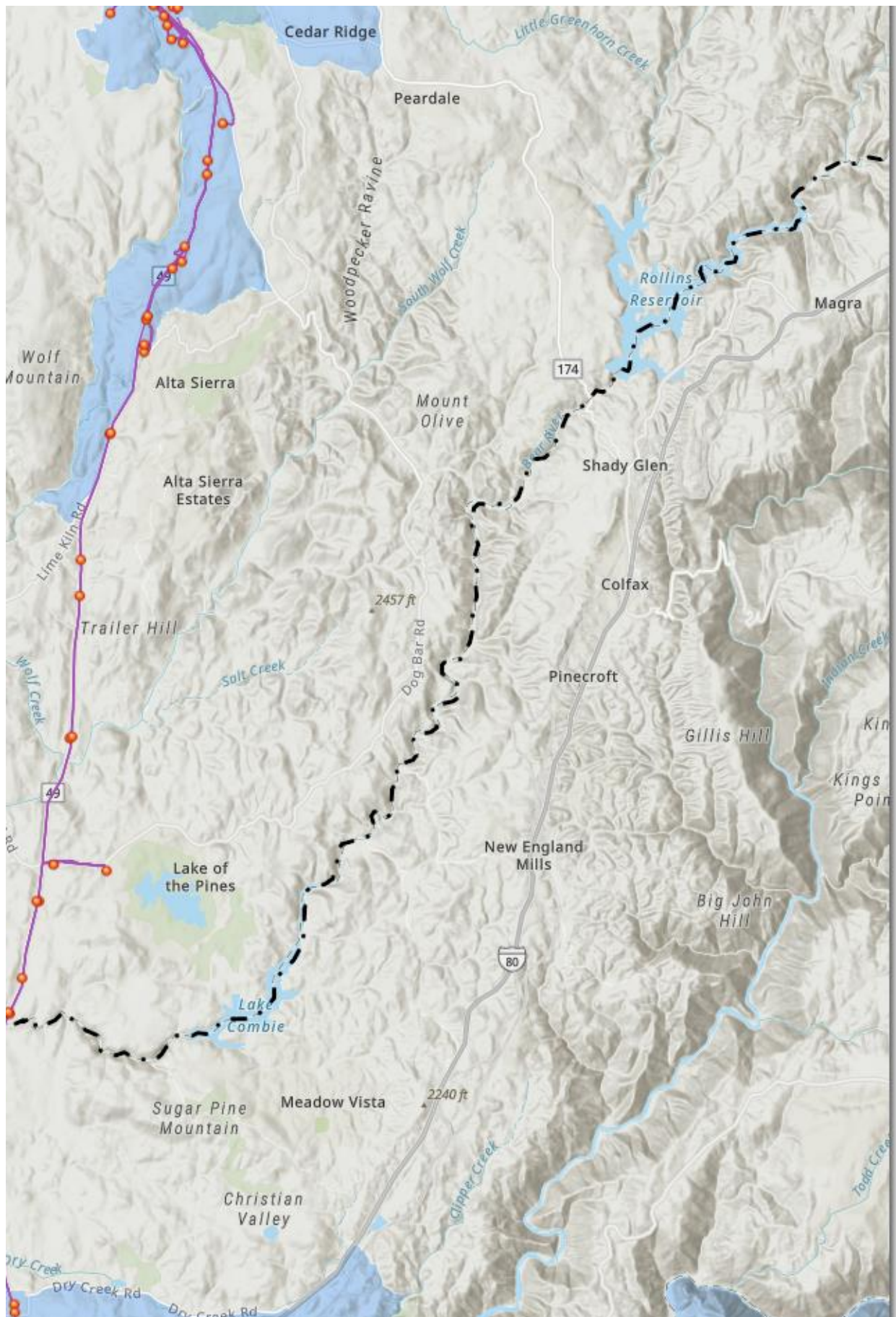


# ject Map

|      |  |  |  |  |  |  |  |  |  |
|------|--|--|--|--|--|--|--|--|--|
| ject |  |  |  |  |  |  |  |  |  |
|      |  |  |  |  |  |  |  |  |  |
|      |  |  |  |  |  |  |  |  |  |













































































































































































































































































































































































































































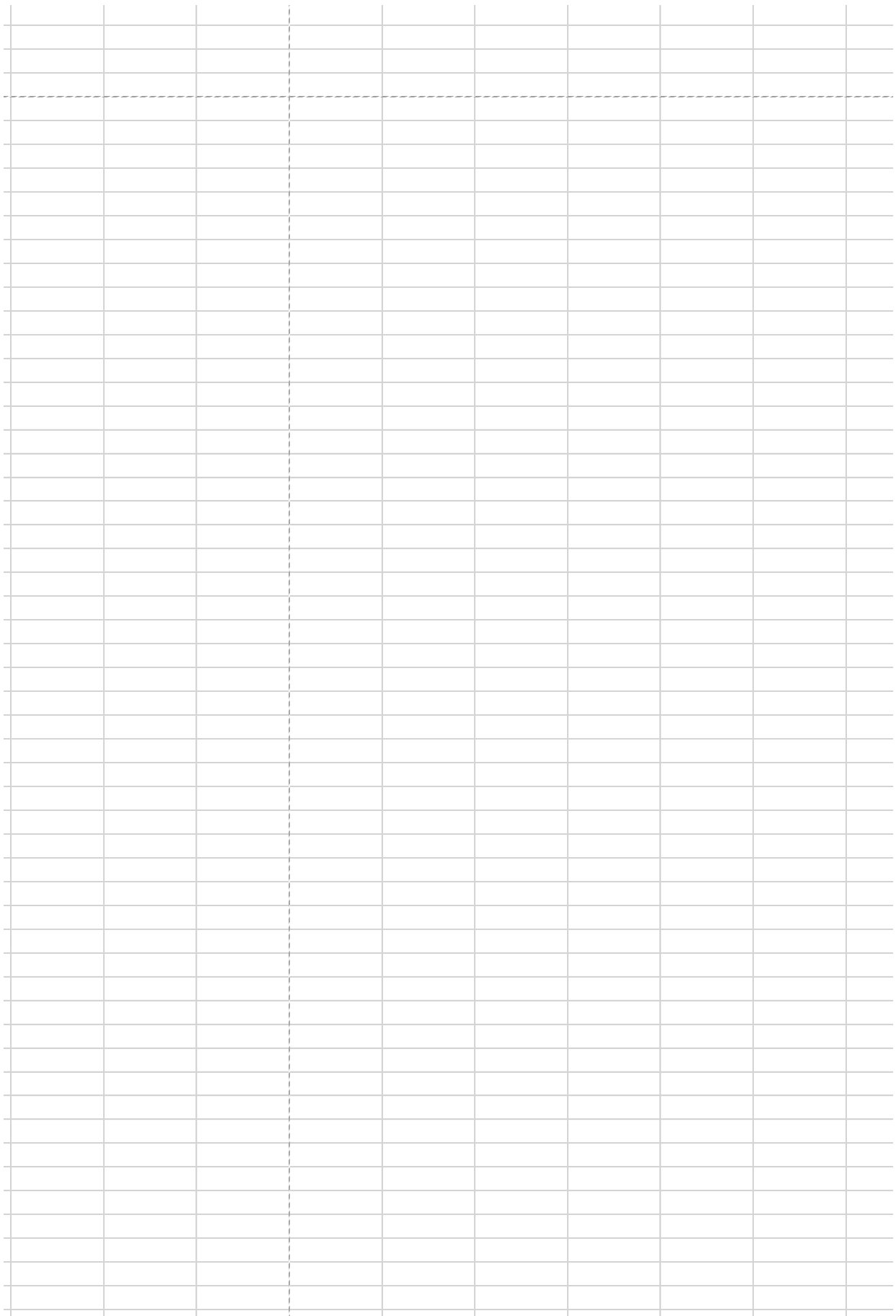










































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































































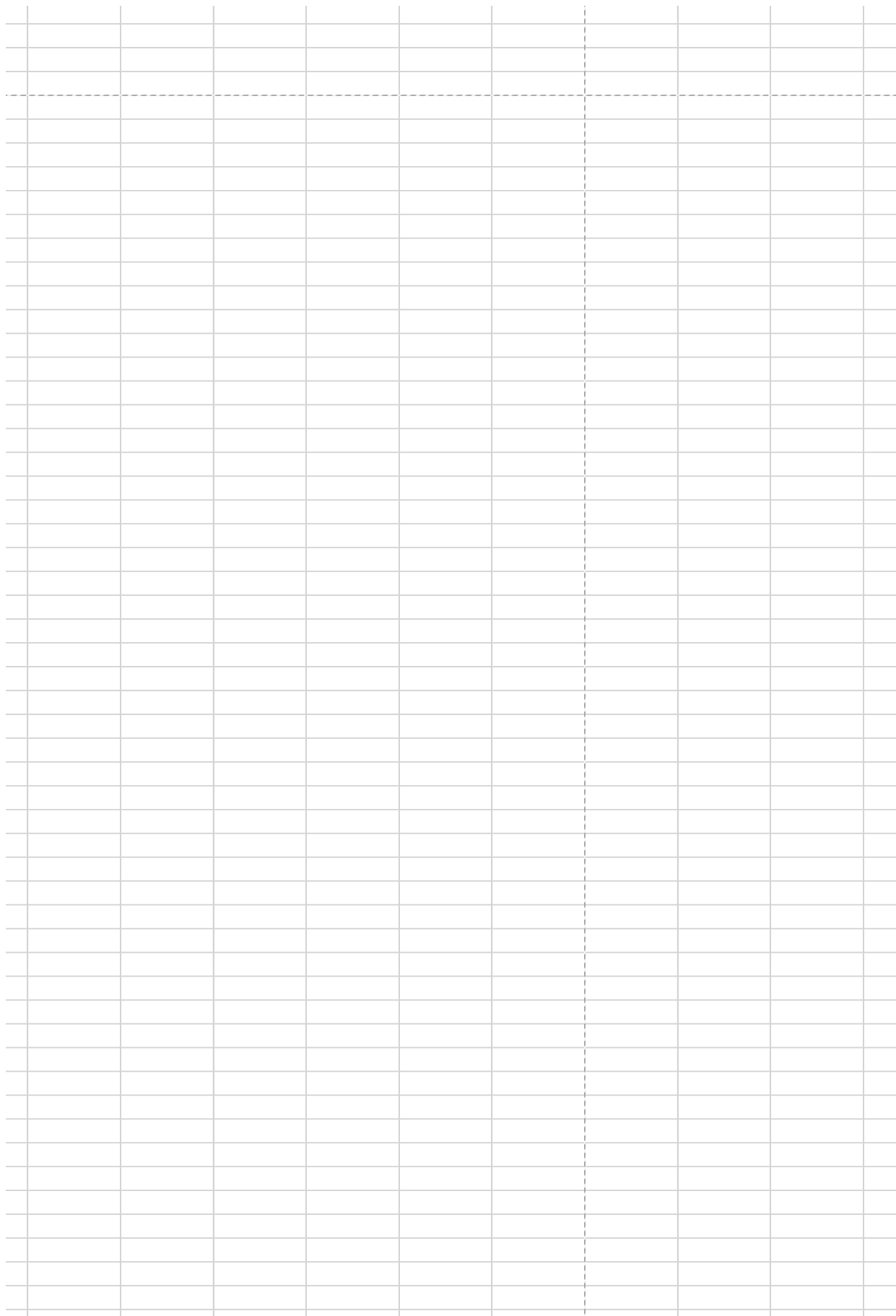






























































































































































































































































































































































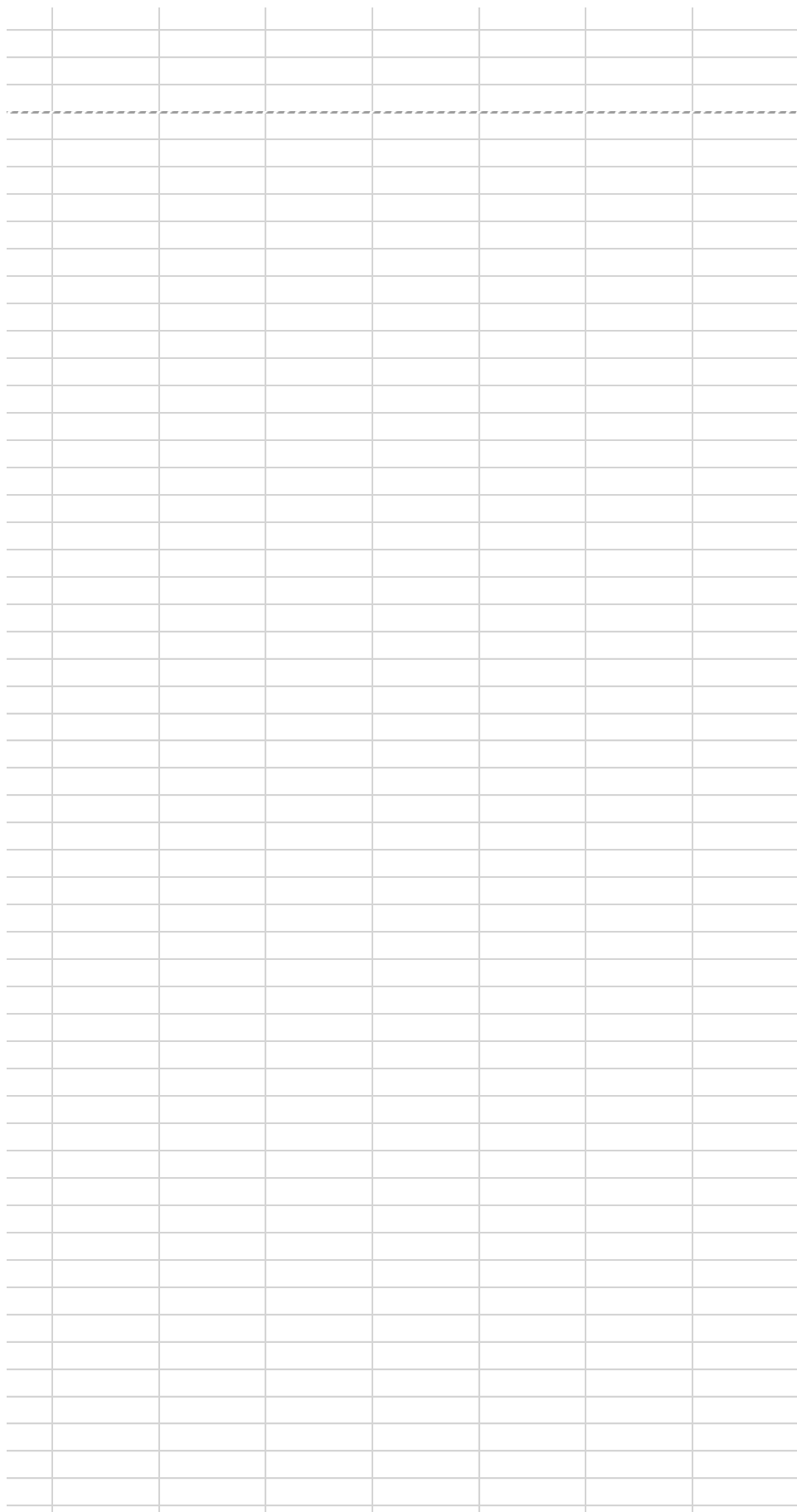




































California Air Resources Board  
 Benefits Calculator Tool for the  
 Low Carbon Transit Operations Program  
 California Climate Investments

**Step 3: Review the Estimated GHG Emission Reductions for the Proposed Project**

|                      |   |
|----------------------|---|
| <b>Project Name:</b> | Nevada County Connects Low-Income Pass Fare Subsidy Program |
|----------------------|---|

| <b>Co-benefits and Key Variables Summary</b>                |           |
|---|-----------|
| <b>LCTOP GGRF Funds</b>                                     |           |
| Local Diesel PM Emission Reductions (lbs)                   | 0         |
| Local NO <sub>x</sub> Emission Reductions (lbs)             | 23        |
| Local PM <sub>2.5</sub> Emission Reductions (lbs)           | 5         |
| Local ROG Emission Reductions (lbs)                         | 4         |
| Remote Diesel PM Emission Reductions (lbs)                  | 0         |
| Remote NO <sub>x</sub> Emission Reductions (lbs)            | 0         |
| Remote PM <sub>2.5</sub> Emission Reductions (lbs)          | 0         |
| Remote ROG Emission Reductions (lbs)                        | 0         |
| Passenger VMT Reductions (miles)                            | 119,919   |
| Fossil Fuel Use Reductions (gallons)                        | 4,017     |
| Fossil Fuel Energy Use Reductions (kWh)                     | 0         |
| Renewable Energy Generated (kWh)                            | 0         |
| Travel Cost Savings (\$)                                    | -\$18,099 |
| Energy and Fuel Cost Savings (\$)                           | \$0       |
| <b>Additional California Climate Investments Program(s)</b> |           |
| Local Diesel PM Emission Reductions (lbs)                   | 0         |
| Local NO <sub>x</sub> Emission Reductions (lbs)             | 0         |
| Local PM <sub>2.5</sub> Emission Reductions (lbs)           | 0         |
| Local ROG Emission Reductions (lbs)                         | 0         |
| Remote Diesel PM Emission Reductions (lbs)                  | 0         |
| Remote NO <sub>x</sub> Emission Reductions (lbs)            | 0         |
| Remote PM <sub>2.5</sub> Emission Reductions (lbs)          | 0         |
| Remote ROG Emission Reductions (lbs)                        | 0         |
| Passenger VMT Reductions (miles)                            | 0         |
| Fossil Fuel Use Reductions (gallons)                        | 0         |
| Fossil Fuel Energy Use Reductions (kWh)                     | 0         |
| Renewable Energy Generated (kWh)                            | 0         |
| Travel Cost Savings (\$)                                    | \$0       |
| Energy and Fuel Cost Savings (\$)                           | \$0       |
| <b>Total California Climate Investments</b>                 |           |
| Local Diesel PM Emission Reductions (lbs)                   | 0         |
| Local NO <sub>x</sub> Emission Reductions (lbs)             | 23        |
| Local PM <sub>2.5</sub> Emission Reductions (lbs)           | 5         |
| Local ROG Emission Reductions (lbs)                         | 4         |
| Remote Diesel PM Emission Reductions (lbs)                  | 0         |
| Remote NO <sub>x</sub> Emission Reductions (lbs)            | 0         |
| Remote PM <sub>2.5</sub> Emission Reductions (lbs)          | 0         |
| Remote ROG Emission Reductions (lbs)                        | 0         |
| Passenger VMT Reductions (miles)                            | 119,919   |



|   |           |
|---|-----------|
| Fossil Fuel Use Reductions (gallons)    | 4,017     |
| Fossil Fuel Energy Use Reductions (kWh) | 0         |
| Renewable Energy Generated (kWh)        | 0         |
| Travel Cost Savings (\$)                | -\$18,099 |
| Energy and Fuel Cost Savings (\$)       | \$0       |



California Air Resources Board  
 Job Co-benefit Modeling Tool  
 California Climate Investments

|              |
|--------------|
| Project Name |
|--------------|

|  |     |
|--|-----|
| Total Full-time Equivalent Jobs Supported by Project Budget          | 3.2 |
| Total Full-time Equivalent Jobs Supported by Project GGRF Funds      | 3.2 |
| Full-time Equivalent Jobs Directly Supported by Project GGRF Funds   | 2.4 |
| Full-time Equivalent Jobs Indirectly Supported by Project GGRF Funds | 0.3 |
| Full-time Equivalent Induced Jobs Supported by Project GGRF Funds    | 0.5 |

**Note:**

It is not appropriate to directly compare the job estimates from this Job Co-benefit Modeling Tool to the GGRF project dollars. California Climate Investments facilitate greenhouse gas emission reductions and deliver a suite of economic, environmental, and public health co-benefits, including job co-benefits. A different mix of spending on materials, equipment, and labor is expected across various California Climate Investments project types and match funding arrangements. As such, some project types will support more jobs than others.



California Air Resources Board  
 Benefits Calculator Tool for the  
 Low Carbon Transit Operations Program  
 California Climate Investments

**Step 3: Review the Estimated GHG Emission Reductions for the Proposed Project**

|                      |   |
|----------------------|---|
| <b>Project Name:</b> | Nevada County Connects Low-Income Pass Fare Subsidy Program |
|----------------------|---|

| <b>Project Information</b>                 |            |
|--|------------|
| FY 2022-23 LCTOP GGRF Funds Requested (\$) | \$ 138,749 |
| Total LCTOP GGRF Funds (\$)                | \$ 138,749 |
| Total GGRF Funds (\$)                      | \$ 138,749 |
| Non-GGRF Leveraged Funds (\$)              | \$ -       |
| Total Funds (\$)                           | \$ 138,749 |

| <b>GHG Summary</b>  |     |
|---|-----|
| Total FY 2022-23 LCTOP GHG Emission Reductions (MTCO <sub>2</sub> e)                          | 49  |
| Total LCTOP GHG Emission Reductions (MTCO <sub>2</sub> e)                                     | 49  |
| Total GHG Emission Reductions (MTCO <sub>2</sub> e)   | 49  |
| Total GHG Emission Reductions per FY 2022-23 LCTOP GGRF Funds (MTCO <sub>2</sub> e/\$million) | 354 |
| Total GHG Emission Reductions per Total GGRF Funds (MTCO <sub>2</sub> e/\$million)            | 354 |