

REVISIONS			
NO.	DESCRIPTION	BY	DATE



**NEVADA COUNTY**  
DEPARTMENT OF PUBLIC WORKS  
DESIGN/CONSTRUCTION DIVISION



**GOLD COUNTRY STAGE  
BUS STOP IMPROVEMENTS**  
DEMOLITION PLAN

DRAWING: C889816PLN  
DESIGNED: RP  
DRAWN: RP  
CHECKED: PP  
JOB NO: 889816  
DATE: MAR 2021

SHEET  
**1**  
OF 3 SHEETS

**DEMOLITION NOTES:**

- ① SAW CUT AND REMOVE EXISTING CONCRETE CURB
- ② SAW CUT AND REMOVE EXISTING AC ROAD
- ③ SAW CUT AND REMOVE EXISTING PCC SIDEWALK
- ④ AREA TO BE USED TO FORM UP PROPOSED PCC TURN-OUT
- ⑤ REMOVE TREE

CONTRACTOR IS RESPONSIBLE FOR RELOCATION OF ANY IRRIGATION EQUIPMENT WITHIN ALL EXISTING LANDSCAPE AREAS AFFECTED BY NEW CONSTRUCTION OF BUS SHELTER PAD SIDEWALK AND TURN-OUT.

PROJECT CONTROL PTS.

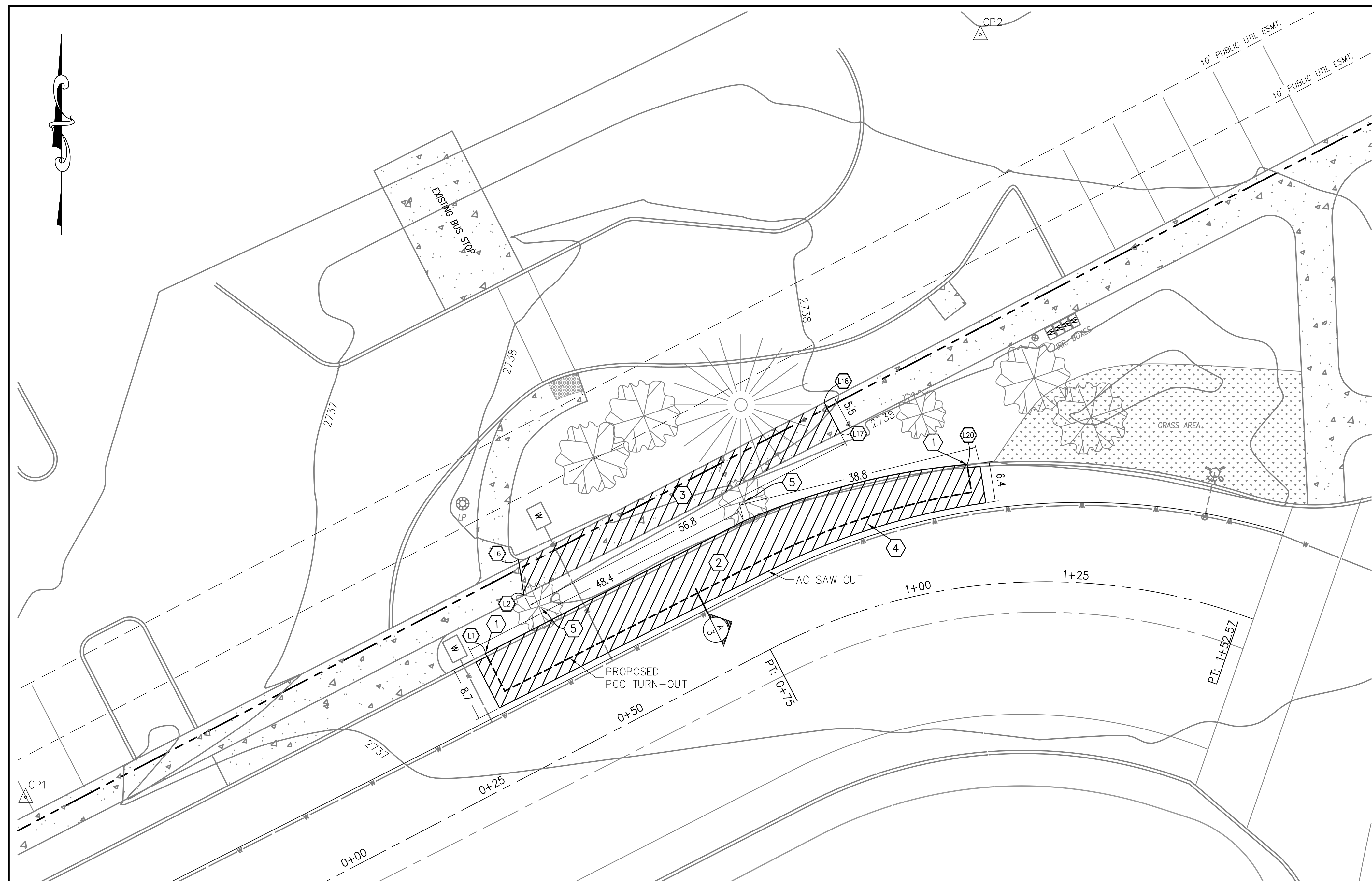
	NORTHING	EASTING	ELEV
CP1 Nail	10,745.588	5,532.323	2732.87'
CP2 Nail	10,867.503	5,684.973	2736.05'
CP3 Nail	10,756.515	5,807.771	2735.83'

BUS STOP LAYOUT PTS.

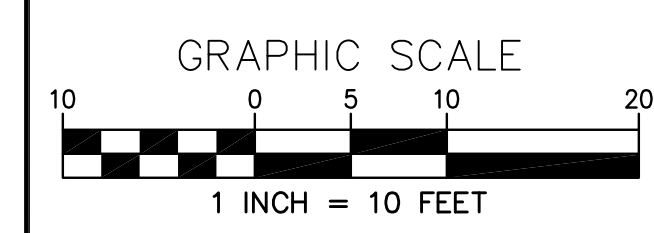
STATION	DISTANCE	NORTHING	EASTING
L1	0+34.3	20.44'	10768.5293
L2	0+44.1	25.90'	10777.8942
L6	0+45.9	31.39'	10783.6168
L17	0+95.1	28.24'	10803.8196
L18	0+94.3	33.60'	10808.6844
L20	1+10.3	19.43'	10798.7839

EXISTING UTILITY NOTES:

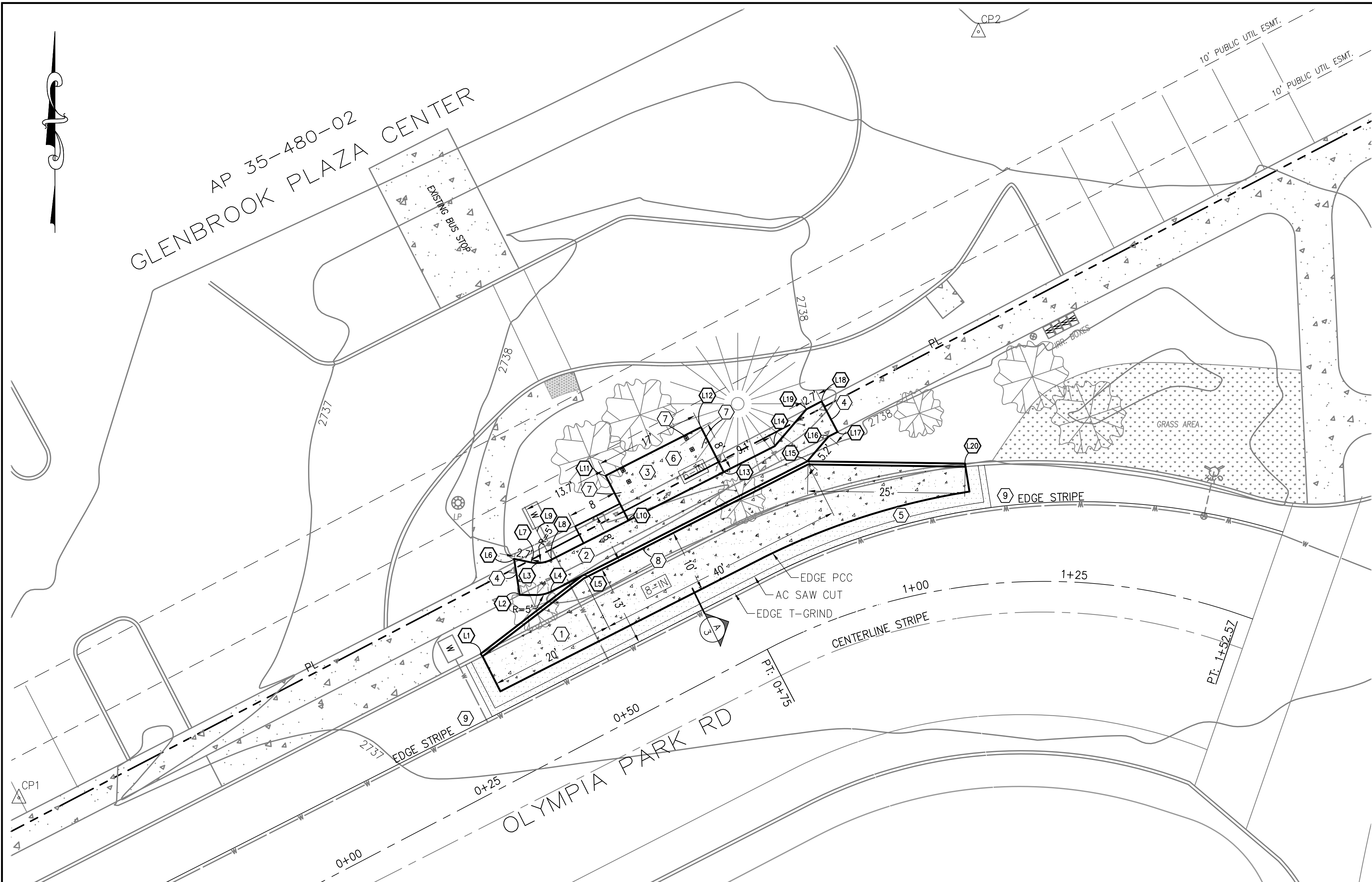
- ALL UNDERGROUND UTILITIES SHOWN ARE FROM RECORD DATA AND ARE SCHEMATIC ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE THE FINAL DETERMINATIONS AS TO THE EXISTENCE, EXACT LOCATION AND DISPOSITION OF ALL EXISTING UTILITIES PRIOR TO COMMENCING WORK.
- CONTRACTOR SHALL CONTACT U.S.A. (UNDERGROUND SERVICE ALERT) AT LEAST 48 HOURS PRIOR TO EXCAVATING IN ANY AREA WHERE UNDERGROUND UTILITIES ARE LOCATED.



**LEGEND:**  
 DEMOLITION AREA







**LEGEND:**

- NEW CONCRETE
- NEW AC PAVEMENT

**GRAPHIC SCALE**

**KEY NOTES**

- ① CONSTRUCT CONCRETE TURNOUT, SEE SECTION A
- ② CONSTRUCT CONCRETE SIDEWALK, SEE SECTION A
- ③ CONSTRUCT CONCRETE PAD, SEE SECTION A
- ④ CONFORM TO EXISTING SIDEWALK
- ⑤ CONSTRUCT AC INFILL TO CONFORM TO NEW CONCRETE TURNOUT AND CONFORM TO EXISTING AC WITH T-GRIND, SEE SECTION A
- ⑥ CONTRACTOR SHALL PURCHASE AND INSTALL NEW BUS SHELTER – TOLAR SIERRA DOME ROOF NON-ADVERTISING TRANSIT SHELTER (13NALDPM) WITH RAL 3018 (STRAWBERRY RED) ROOF, AND RAL 7000 (SQUIRREL GREY) WALLS AND BENCH OR APPROVED EQUAL TRANSIT SHELTER. DETAILS FOR 13NALDPM ARE INCLUDED IN SPECIAL PROVISIONS.
- ⑦ INSTALL SHELTER TO CONCRETE PAD ACCORDING TO MANUFACTURER'S REQUIREMENTS FOR APPROVED SHELTER.
- ⑧ PAINT CURB RED ALONG ENTIRE LENGTH OF TURN-OUT.
- ⑨ RESTRIPE EDGE STRIPE TO 10- FEET BEYOND EACH SIDE OF THE WORK

**CONTRACTOR IS RESPONSIBLE FOR RELOCATION OF ANY IRRIGATION EQUIPMENT WITHIN ALL EXISTING LANDSCAPE AREAS AFFECTED BY NEW CONSTRUCTION OF BUS SHELTER PAD SIDEWALK AND TURN-OUT**

**PROJECT CONTROL PTS.**

	NORTHING	EASTING	ELEV
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**BUS STOP LAYOUT PTS.**

STATION	DISTANCE	NORTHING	EASTING	
L1	0+34.3	20.44'	10768.5293	5605.8162
L2	0+44.1	25.90'	10777.8942	5612.0119
L3	0+48.8 RAD PT.	20.06'	10782.8725	5614.7109
L4	0+49.0	24.25'	10778.4156	5616.9772
L5	0+54.2	23.99'	10780.9408	5621.9432
L6	0+45.9	31.39'	10783.6168	5611.1051
L7	0+48.1	29.67'	10783.0636	5613.7697
L8	0+51.2	28.54'	10783.5023	5617.0525
L9	0+51.2 RAD PT.	33.54'	10787.9592	5614.7862
L10	0+64.9	2839'	10789.7323	5629.3044
L11	0+64.9	36.39'	10796.8634	5625.6783
L12	0+80.1	36.39'	10804.5688	5640.8317
L13	0+80.3	28.39'	10797.4378	5644.4578
L14	0+87.3	29.13'	10801.5645	5652.5734
L15	0+90.5	25.16'	10799.1485	5657.7500
L16	0+93.9	27.91'	10803.0865	5661.1375
L17	0+95.1	28.24'	10803.8196	5662.5667
L18	0+94.3	33.60'	10808.6844	5660.0162
L19	0+92.3	33.08'	10807.4256	5657.6152
L20	1+10.3	19.43'	10798.7839	5682.8462

**EXISTING UTILITY NOTES:**

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NOTES

ALL WORK SHALL BE DONE TO THE CITY OF GRASS VALLEY CONSTRUCTION STANDARDS

1) PREFERRED CONSTRUCTION IS FOR THE TURNOUT, CURB, SIDEWALK AND PAD TO BE PLACED MONOLITICALLY. IF THE CONCRETE IS PLACED WITH A COLD JOINT BETWEEN THE BOTTOM OF THE CURB AND THE TURNOUT SLAB, THEN CURB SHALL BE DOWELED WITH 8" LONG GRADE 60 REBAR AT 4-FOOT CENTERS.

2) AGGREGATE BASE IS NOT REQUIRED IN THE STRUCTURAL SECTION FOR CONCRETE SIDEWALK. 3/4-INCH AGGREGATE BASE MAY BE SUBSTITUTED FOR A COMPACTED SOIL SUBGRADE AT CONTRACTOR'S DISCRETION AND SHALL BE PROCESSED TO 95% RELATIVE COMPACTION. IF COMPACTED SOIL SUBGRADE IS USED, 95% RELATIVE COMPACTION TO A DEPTH OF 6-INCHES MINIMUM SHALL BE ACHIEVED.

3) CONCRETE NOTES  
ALL CONCRETE SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 73 OF THE CALTRANS STANDARD SPECIFICATIONS AND SHALL HAVE A 28-DAY COMPRESSIVE STRENGTH OF 3,000 PSI OR GREATER WITH A 4-INCH SLUMP UNLESS SPECIFIED OTHERWISE.

THE SUPPLIER SHALL PROVIDE CERTIFICATION THAT ANY CONCRETE FURNISHED CONFORMS TO THESE SPECIFICATIONS. THE MAXIMUM ALLOWABLE HOLDING TIME BEFORE CONCRETE PLACEMENT SHALL BE 90-MINUTES FROM BATCH PLANT.

CONCRETE SHALL NOT BE PLACED OR FINISHED IN THE RAIN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SCHEDULE CONSTRUCTION OPERATIONS ACCORDINGLY.

ALL GUTTERS SHALL BE FLOW TESTED WITH WATER TO ASSURE PROPER DRAINAGE. FOLLOWING CONCRETE FINISHING, NO WATER SHALL POND IN THE GUTTER PAN.

ALL CONCRETE SURFACES SHALL BE COMPLETED WITH A MEDIUM BROOM FINISH UNLESS SPECIFIED OTHERWISE. SURFACES TO BE USED BY PEDESTRIANS SHALL BE BROOMED TRANSVERSELY TO THE DIRECTION OF TRAVEL. BLEMISHES AND ALIGNMENT TOLERANCES NOT CONFORMING TO THE CALTRANS STANDARD SPECIFICATIONS SHALL BE CAUSE FOR REJECTION OF THE WORK. NO STAMPS ADVERTISING CONSTRUCTION COMPANIES OR ANY OTHER PRIVATE CONCERNS SHALL BE PLACED IN THE CONCRETE.

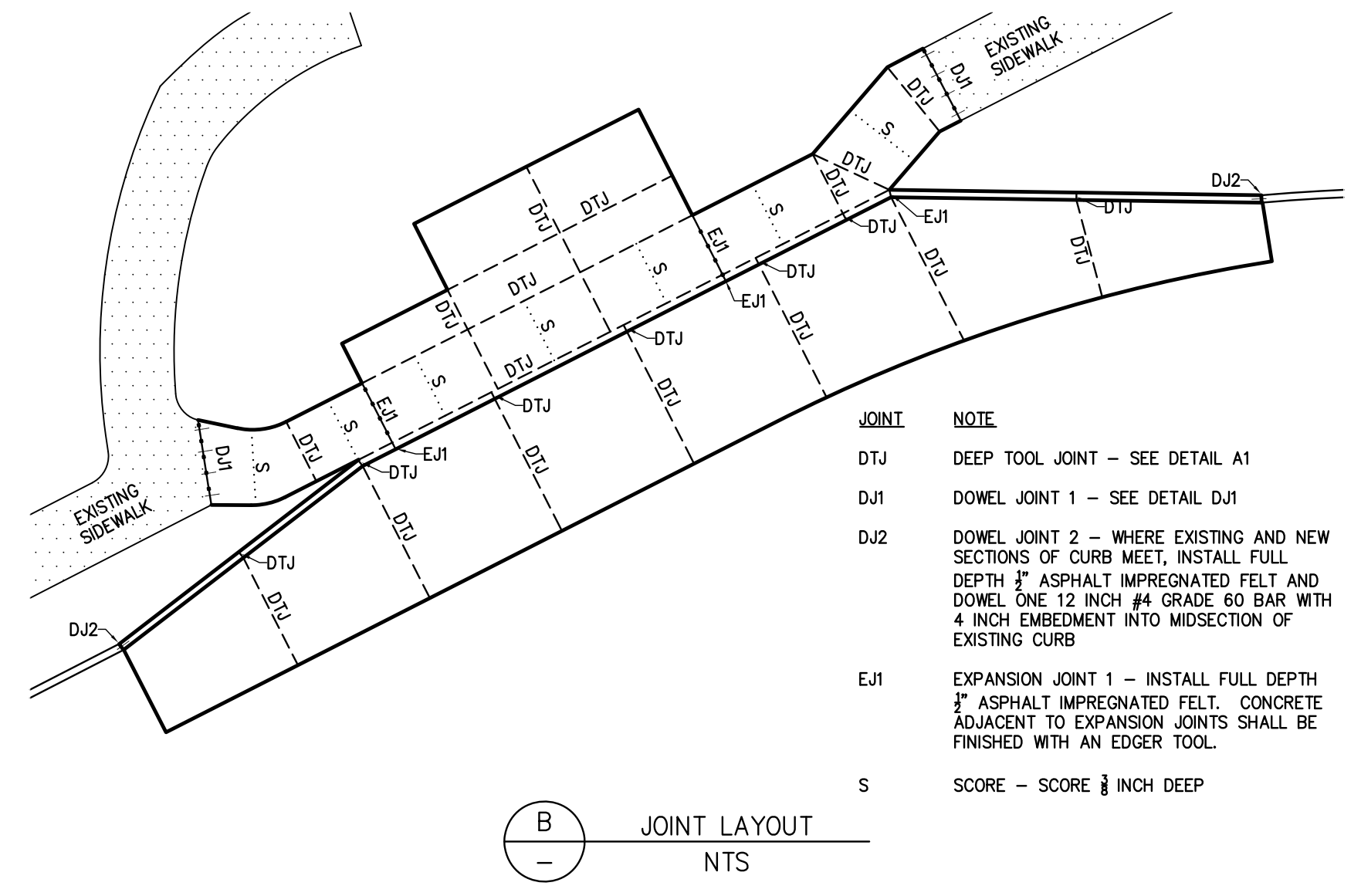
4) AFTER TURNOUT CONSTRUCTION IS COMPLETE, PROVIDE A SMOOTH EDGE FOR PLACEMENT OF ASPHALT PATCH BY SAWCUTTING 2-INCHES DEEP AND THEN GRINDING ASPHALT 2-INCHES DEEP AND 1-FOOT WIDE. EDGES SHALL BE COATED WITH TYPE 3 SEAL COAT OR SEAL COAT WITH SAND BEFORE PLACEMENT OF ASPHALT PATCH..

5) ALL AGGREGATE BASE SHALL BE 3/4-INCH MAXIMUM, CLASS 2 AB COMPLYING WITH THE APPLICABLE SECTIONS OF THE CALTRANS STANDARD SPECIFICATIONS. AGGREGATE BASE SHALL BE MOISTURE CONDITIONED TO OPTIMUM MOISTURE CONTENT AND COMPACTED TO 95% RELATIVE COMPACTION. AGGREGATE BASE MAY CONTAIN RECYCLED ASPHALT CONCRETE PAVEMENT AND CONCRETE. THE RECYCLED MATERIAL SHALL BE CLEAN AND NOT CONTAIN ANY DELETERIOUS MATERIALS INCLUDING WOOD, PLASTIC OR METAL. THE AGGREGATE BASE SHALL COMPLY WITH ALL THE APPLICABLE QUALITY REQUIREMENTS FOR CLASS 2 AB. AC GRINDINGS SHALL NOT BE USED DIRECTLY FOR AGGREGATE BASE.

6) TRENCH BACKFILL SHALL BE 3/4-INCH MAXIMUM CLASS 2 AB PROCESSED TO 95% RELATIVE COMPACTION OR 2-SACK CEMENT SLURRY.

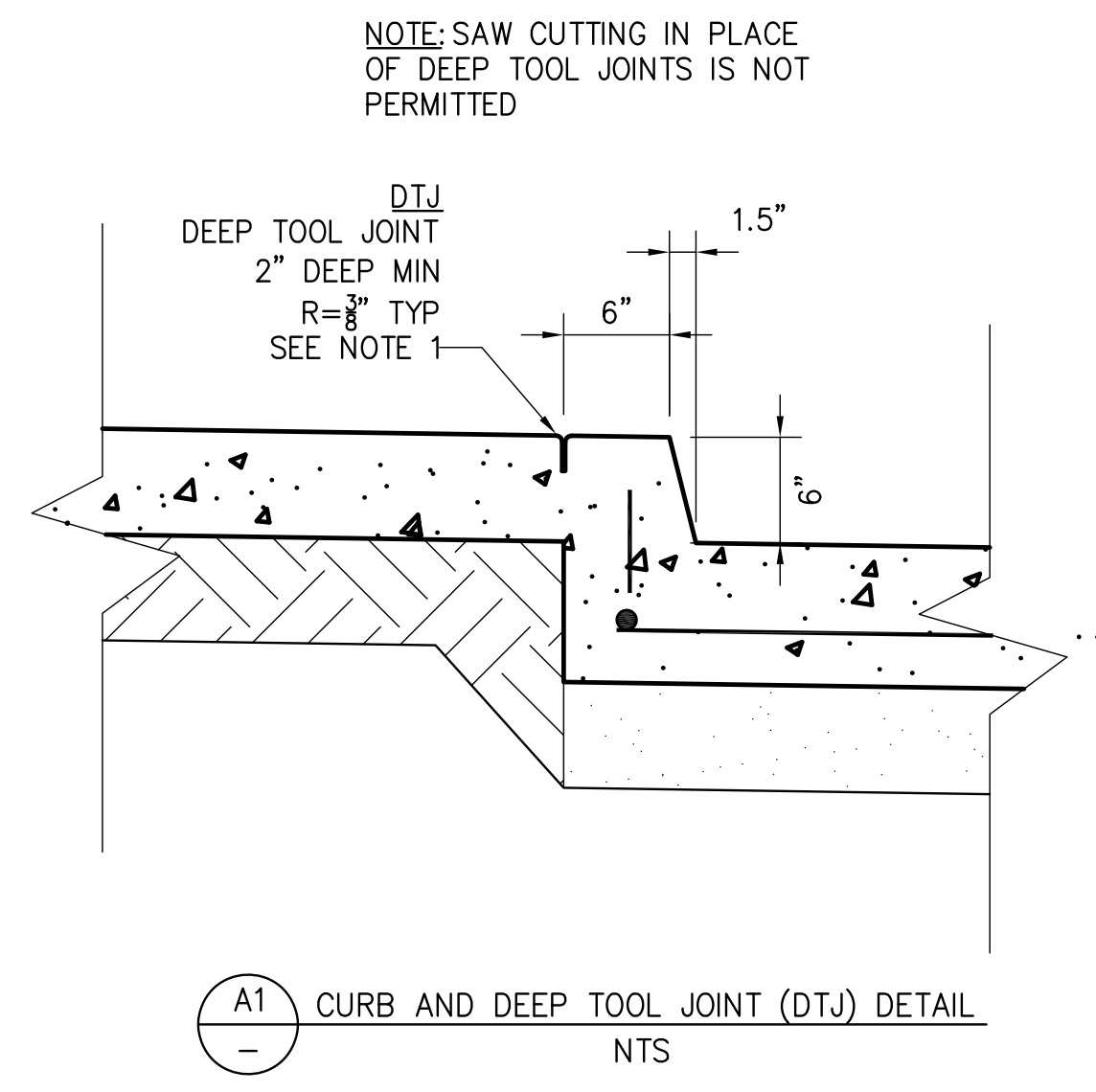
7) ALL ASPHALT CONCRETE SHALL BE INSTALLED ACCORDING TO SECTION 39 OF THE CALTRANS SPECIFICATIONS.

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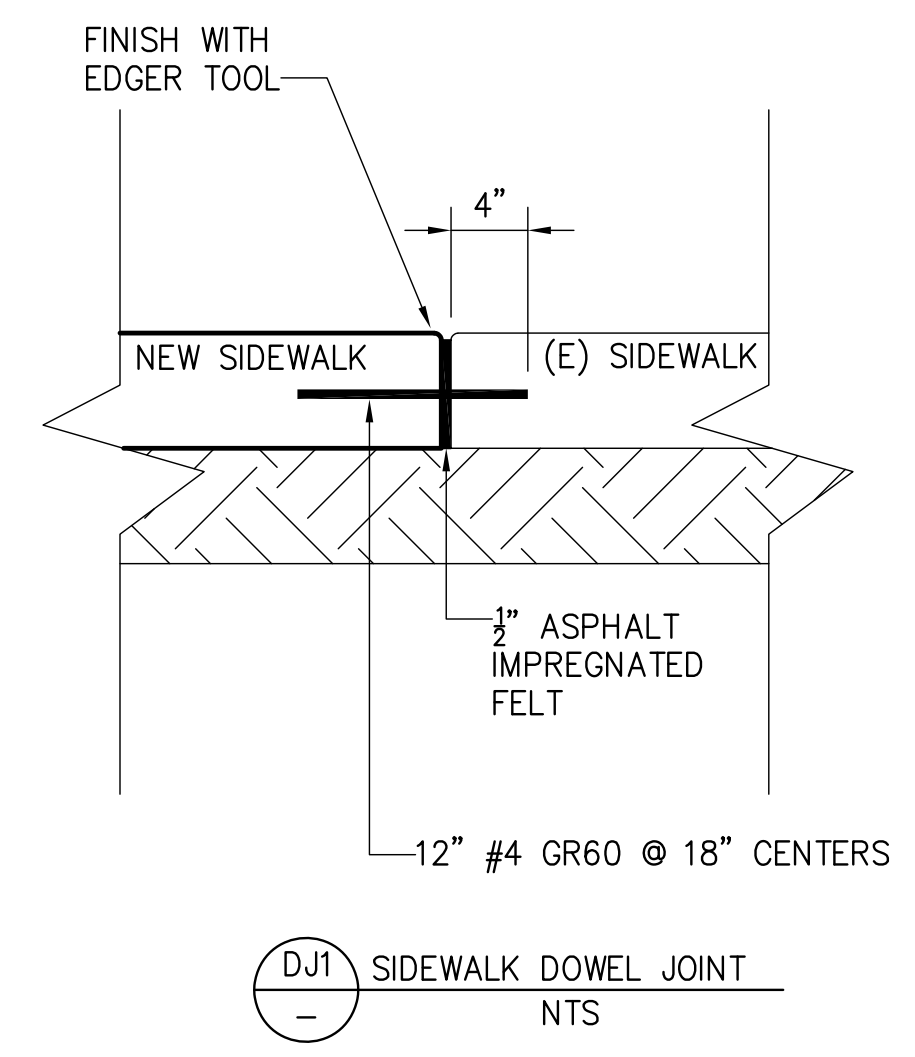


- JOINT NOTE
- DTJ DEEP TOOL JOINT - SEE DETAIL A1
  - DJ1 DOWEL JOINT 1 - SEE DETAIL DJ1
  - DJ2 DOWEL JOINT 2 - WHERE EXISTING AND NEW SECTIONS OF CURB MEET, INSTALL FULL DEPTH 3" ASPHALT IMPREGNATED FELT AND DOWEL ONE 12 INCH #4 GRADE 60 BAR WITH 4 INCH EMBEDMENT INTO MIDSECTION OF EXISTING CURB
  - EJ1 EXPANSION JOINT 1 - INSTALL FULL DEPTH 3" ASPHALT IMPREGNATED FELT. CONCRETE ADJACENT TO EXPANSION JOINTS SHALL BE FINISHED WITH AN EDGER TOOL.
  - S SCORE - SCORE 1/8 INCH DEEP

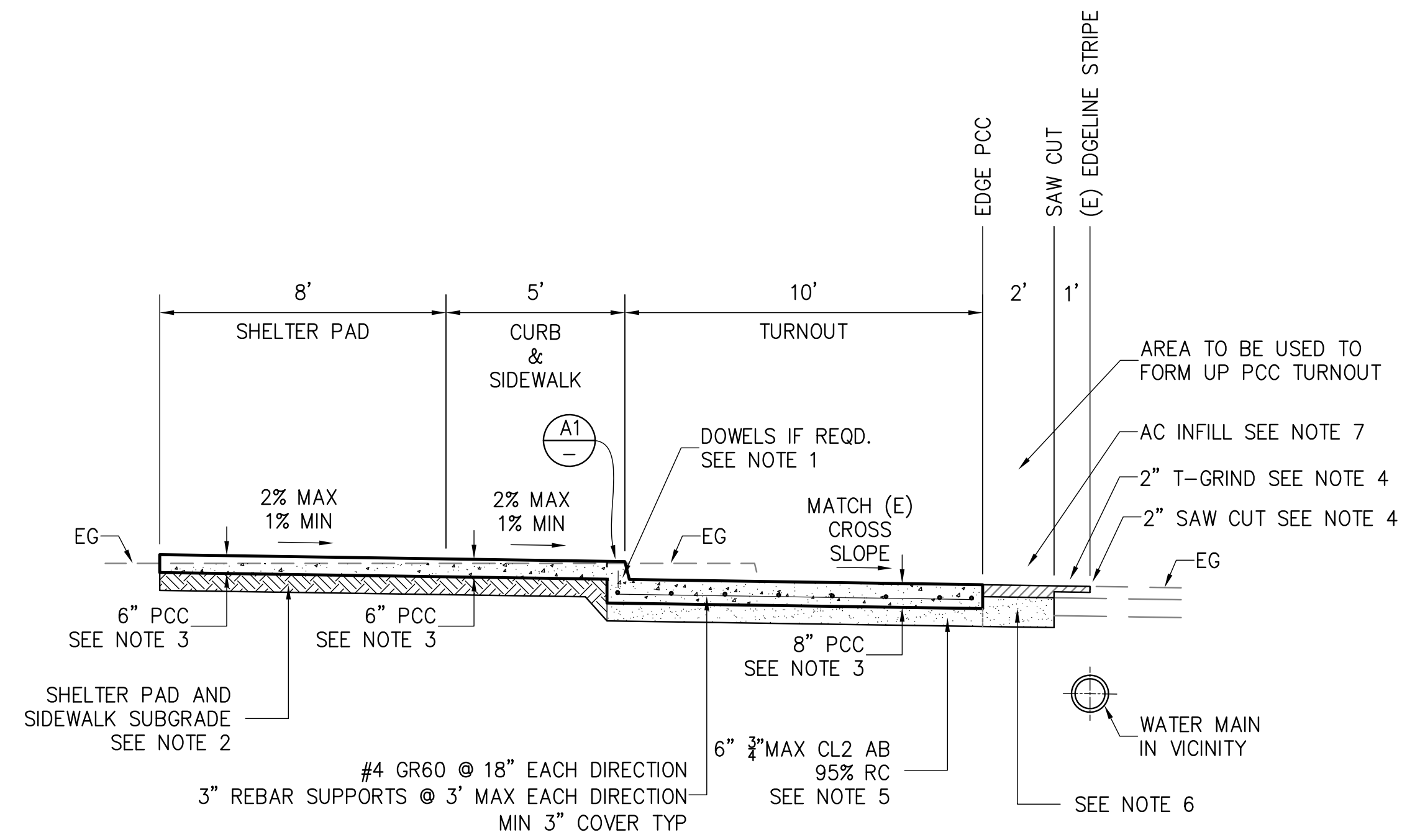
(B) JOINT LAYOUT  
NTS



(A1) CURB AND DEEP TOOL JOINT (DTJ) DETAIL  
NTS



(DJ1) SIDEWALK DOWEL JOINT  
NTS



(A) SECTION  
NTS