

# CONSTRUCTION PLANS FOR:

NEVADA COUNTY AIRPORT  
NEVADA COUNTY, CALIFORNIA

AIP NO. 3-06-0095-\_\_-2025

## RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4 TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD

FEBRUARY 2025

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NEVADA COUNTY AIRPORT

APPROVED: \_\_\_\_\_ DATE: \_\_\_\_\_  
KEVIN EDWARDS, AIRPORT MANAGER

APPROVED: \_\_\_\_\_ DATE: \_\_\_\_\_

DESIGNED BY:

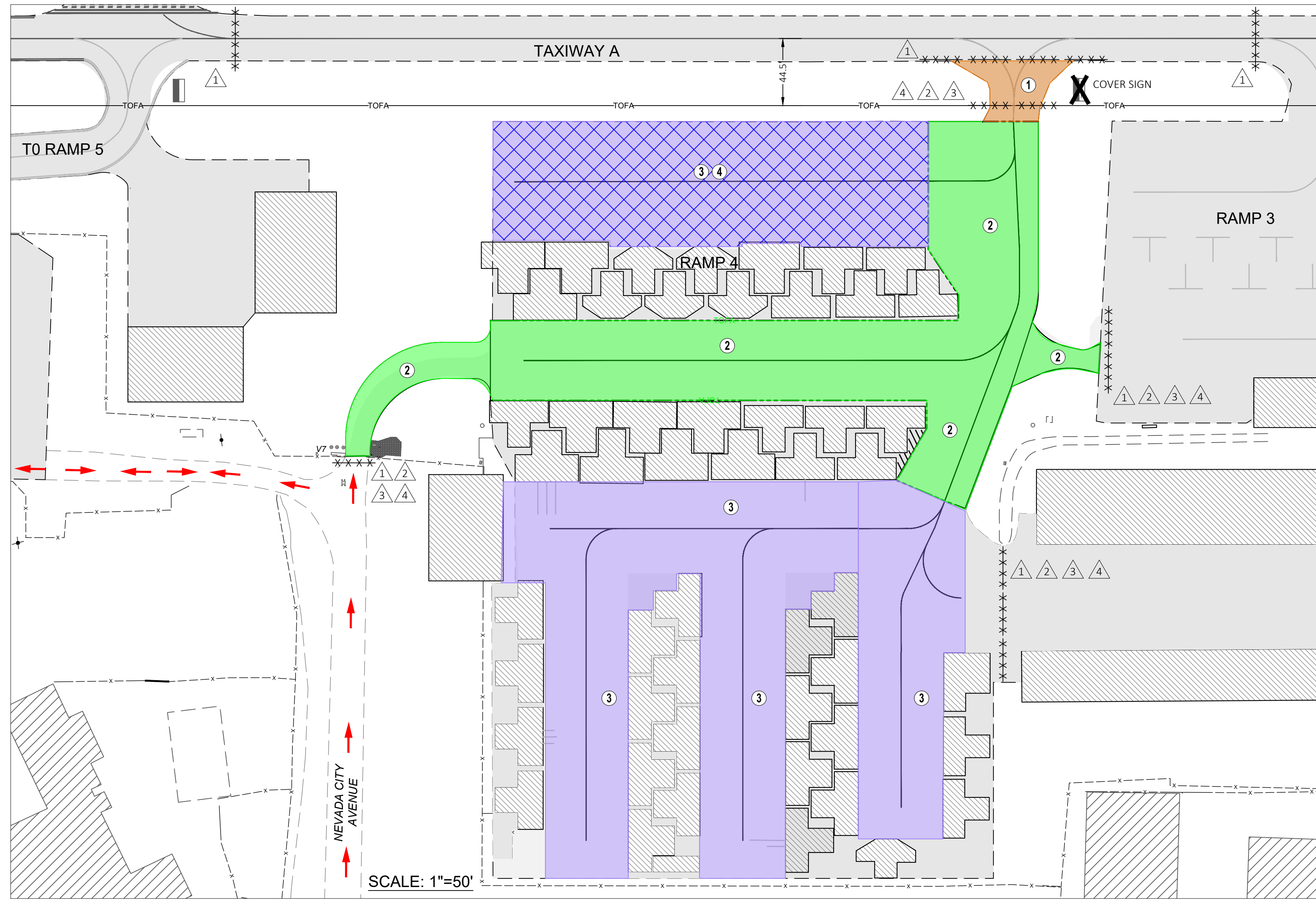
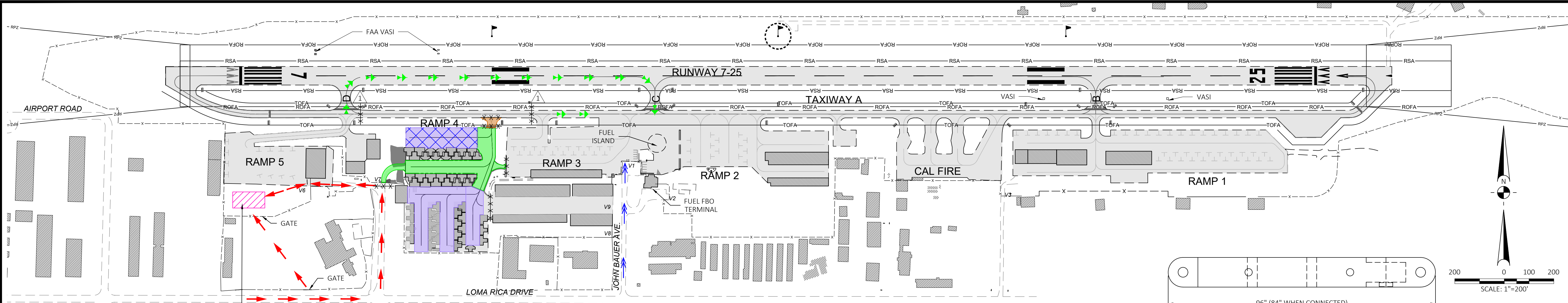


6125 KING ROAD, SUITE 201 · LOOMIS, CALIFORNIA 95650 · (916) 652-4725





Q:\38 NEVADA COUNTY\07 RAMP\BID SET\3607.02 CSPP.DWG PLOTTED BY David Baharar 3/3/2025 11:30 AM



#### LEGEND

- 1 RAMP 4 WORK PHASE 1
- 2 RAMP 4 WORK PHASE 2
- 3 4 RAMP 4 WORK PHASE 3 & 4
- CONTRACTOR'S STORAGE AREA
- EXISTING BUILDING
- EXISTING EDGE OF PAVEMENT
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- RUNWAY PROTECTION ZONE
- TAXIWAY OBJECT FREE AREA
- AIRCRAFT ROUTE
- HAUL ROAD
- ARFF ACCESS ROUTE
- VEHICLE GATE IDENTIFICATION
- CONSTRUCTION BARRICADE FOR ASSOCIATED WORK AREA

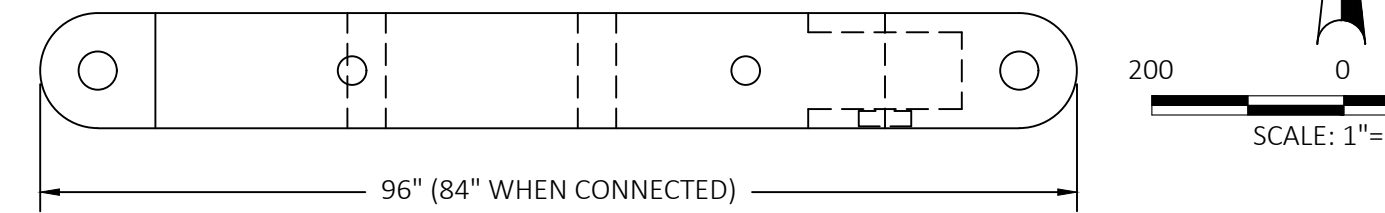
CTAF/UNICOM: 122.725

#### GENERAL NOTES:

- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE SECURITY AND CLEANLINESS OF THE WORK SITE AND CONTRACTOR'S STORAGE AREA AT ALL TIMES.
- CONTRACTOR SHALL MAINTAIN HAUL ROAD AND PAVED SURFACES CLEAN OF DEBRIS AT ALL TIMES. APRON AND TAXIWAYS SHALL BE MAINTAINED BROOM CLEAN.
- AIRPORT OWNER WILL PROVIDE A QUALIFIED FLAGMAN TO MONITOR AIRPORT FREQUENCY 122.725 MHZ AND OBSERVE AIRCRAFT OPERATIONS WHEN CONTRACTOR IS WORKING NEAR ACTIVE TAXIWAYS AND RUNWAYS.
- TAXIWAY AND RAMP CLOSURES SHALL OCCUR FOR EACH PHASE OF WORK. CONTRACTOR SHALL SUBMIT DETAILED WORK SCHEDULE AT PRE-CONSTRUCTION MEETING OR TWO WEEKS PRIOR TO START OF WORK, WHICHEVER IS GREATER. AIRPORT MANAGER WILL ISSUE THE APPROPRIATE NOTAMS FOR TAXIWAY AND RAMP CLOSURES.
- CONTRACTOR SHALL LIMIT ALL CONSTRUCTION VEHICLE ACTIVITY TO THE LIMITS OF THE PROJECT AND THE HAUL ROAD SHOWN ON THE PLANS. NO CONSTRUCTION TRAFFIC WILL BE ALLOWED BEYOND THE LIMITS OF THE WORK AREA.
- CONTRACTOR SHALL FURNISH, ERECT & MAINTAIN LIGHTED BARRICADES AT THE BOUNDARY OF THE WORK AREA TO KEEP VEHICLES AND AIRCRAFT FROM ENTERING THE CONTRACTOR'S WORK AREAS.
- CONTRACTOR SHALL COVER SIGNS TO CLOSED FACILITIES AS INDICATED ON THE CLOSED FACILITY SIGNAGE TABLE ON PHASE BARRICADE PLANS. SIGNS PANELS SHALL BE COVERED SUCH THAT THE PANEL IS NOT VISIBLE DURING DAY OR NIGHT OPERATIONS. COVERS SHALL BE SECURED TO SIGN TO RESIST PROP BLAST AND WEATHER CONDITIONS. ALL SIGN COVERS SHALL BE APPROVED BY RESIDENT ENGINEER.
- CONTRACTOR SHALL PROTECT ALL PROPERTY CORNER MARKERS AND BENCH MARKS FROM DAMAGE.
- CONTRACTOR SHALL VERIFY LOCATION AND PROTECT ALL EXISTING UTILITIES.
- SHOULD CONTRACTOR ENCOUNTER AND DAMAGE A WATERLINE ON THE AIRPORT HE SHALL IMMEDIATELY NOTIFY THE LOCAL FIRE DEPARTMENT AND THE AIRPORT.

#### PHASING NOTES:

- CONTRACTOR SHALL FURNISH, PLACE, MAINTAIN & REMOVE BARRICADES AS REQUIRED.
- ACCESS TO SITE IS THROUGH GATE V7. ACCESS TO THE CONTRACTOR'S STORAGE AND STAGING AREA IS THROUGH GATE V6, SOUTH OF RAMP 5.
- THERE ARE FOUR WORK PHASES FOR THIS PROJECT. WORK PHASE 1 IS REMARK AND DRAINAGE IN THE TAXIWAY A OBJECT FREE AREA. WORK PHASE 2 IS RECONSTRUCT RAMP 4 OUTSIDE OF TAXIWAY A OBJECT FREE AREA. WORK PHASE 3 IS THE CRACK SEAL OF THE RAMP 4 TAXILANES. WORK PHASE 4 IS THE CRACK SEAL/SURFACE SEAL OF RAMP 4 TAXILANES.
- NO WORK SHALL BEGIN UNTIL ALL BARRICADES HAVE BEEN INSTALLED TO CLOSE THE WORK AREA AS SHOWN ON THE PLAN.
- WORK PHASE 1 WILL REQUIRE CLOSURE OF A PORTION OF TAXIWAY A. CLOSURE OF TAXIWAY A MAY REQUIRE AIRCRAFT TO BACK TAXI ON THE RUNWAY IN ORDER TO RELOCATE FROM ONE END OF THE AIRPORT TO THE OTHER.
- CLOSURE OF A PORTION OF TAXIWAY A FOR WORK PHASE 1 WILL ONLY BE IN PLACE DURING WORKING HOURS.
- PHASE 4 WORK AREA SHALL REMAIN CLOSED TO ALL TRAFFIC AFTER COMPLETING SEAL COAT FOR 48 HOURS BEFORE MARKING CAN BE INSTALLED.
- CONTRACTOR SHALL GIVE AIRPORT 2 WEEKS NOTICE PRIOR TO START OF CONSTRUCTION.



10" WATER-FILLED BARRICADE  
NOT TO SCALE

#### BARRICADE NOTES:

- CONTRACTOR WILL FURNISH, INSTALL, AND MAINTAIN PLASTIC WATER-FILLED LIGHTED BARRICADES.
- EACH BARRICADE SHALL HAVE 2 SOLAR-POWERED LIGHTS WITH RED LENSES EACH CONTROLLED BY PHOTOCELLS SUCH THAT THEY ARE ON CONTINUOUS AT NIGHT AND OFF BY DAY.
- BARRICADES SHALL BE SECURED IF NECESSARY TO RESIST MOVEMENT FROM JET BLASTS OR PROP BLAST.
- LOCATE BARRICADES AS SHOWN ON THE PLAN AND DIRECTED BY RESIDENT PROJECT REPRESENTATIVE. SPACING BETWEEN BARRICADES SHALL NOT EXCEED 4 FEET. SEE PHASING PLANS FOR LOCATIONS.
- AT THE COMPLETION OF THE CONSTRUCTION, ALL BARRICADES SHALL BE DRAINED OF WATER AND REMOVED FROM THE AIRPORT.

- IN CASE OF AN AIRCRAFT EMERGENCY THE AREA AROUND THE AIRCRAFT SHALL BE EVACUATED AND NOT REENTERED BY THE CONTRACTOR WITHOUT GIVEN PERMISSION EXCEPT FOR LIFESAVING ACTIVITIES.
- ALL GATES USED BY THE CONTRACTOR SHALL REMAIN CLOSED AT ALL TIMES EXCEPT WHEN AUTHORIZED EQUIPMENT IS ACTUALLY ENTERING THE AIRPORT OR GATE IS CONTINUOUSLY GUARDED BY A FLAGGER, TRAINED BY THE AIRPORT, TO KEEP UNAUTHORIZED PERSONNEL AND WILDLIFE FROM ENTERING THE AIRPORT.
- CONTRACTOR'S STORAGE AND STAGING AREA LOCATION SHALL BE DESIGNATED BY THE AIRPORT MANAGER. CONTRACTOR SHALL CLEAR & GRADE AREA FOR STORAGE. NO SEPARATE PAYMENT FOR CLEARING & GRADING. AREA SHALL BE RESTORED TO ITS ORIGINAL CONDITION ONCE PROJECT IS COMPLETE.
- ALL EQUIPMENT OPERATING DURING DAYLIGHT HOURS SHALL BE EQUIPPED WITH AN ORANGE AND WHITE CHECKERED FLAG OR FLASHING AMBER BEACON. EQUIPMENT OPERATING IN LOW VISIBILITY CONDITIONS, DAWN OR DUSK HOURS SHALL BE EQUIPPED AND USE AN AMBER FLASHING BEACON.
- ALL TRASH SHALL BE PLACED IN WASTE CONTAINERS TO PREVENT THE ATTRACTION OF WILDLIFE. WASTE CONTAINERS SHALL BE EQUIPPED WITH LIDS AND SECURED AT ALL TIMES. NO TRASH OR DEBRIS SHALL BE LEFT ON SITE BY THE CONTRACTOR.
- SEE SHEET 4 FOR BID SCHEDULE IDENTIFICATION DETAILS.

#### SEEDING NOTES:

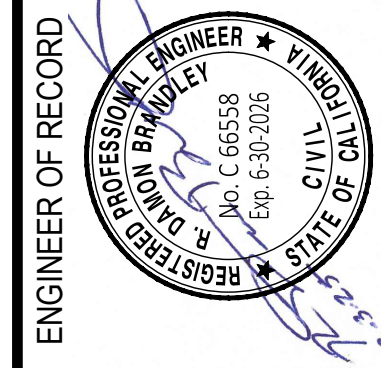
- AT THE CONCLUSION OF ALL WORK, CONTRACTOR SHALL SEED CONTRACTOR'S STORAGE, STAGING AND TEMPORARY STOCKPILE AREA AND ALL DISTURBED GRADING OR EMBANKMENT AREAS. LIMITS OF SEEDING SHALL BE VERIFIED BY RESIDENT ENGINEER. HYDROSEEDING SHALL BE A REQUIRED BMP TO BE INCLUDED IN THE SWPPP. NO ADDITIONAL PAYMENT FOR SURFACE PREPARATION OR SEEDING OF DISTURBED AREAS SHALL BE MADE, COST SHALL BE INCLUDED IN THE COST OF PREPARING AND IMPLEMENTING THE SWPPP. HYDROSEEDING MUST PRODUCE ADEQUATE GROWTH TO SATISFY THE STATE WATER BOARD SUCH THAT THE SWPPP CAN BE CLOSED OUT AFTER THE PROJECT IS COMPLETE.
- SEED MIX SHALL BE:

SEED	RATE OF APPLICATION (LBS/ACRE)
ZORRO ANNUAL FESCUE	6
ROSE CLOVER	9
FERTILIZER MULCH	AS NEEDED TO PROVIDE ADEQUATE GROWTH/STABILIZATION

#### WORK PHASING SCHEDULE

PHASE	CONTRACTOR'S WORK AREA	FACILITY CLOSURE	RESTRICTIONS
1	RAMP 4 INSIDE THE OBJECT FREE AREA OF TAXIWAY A	PORTION OF TAXIWAY A FROM TAXIWAY D TO RAMP 3 RAMP 4	MINIMUM NOTICE OF 2 WEEKS PRIOR TO CLOSURE. WORK IN PHASE 1, 2, 3 & 4 MAY BE DONE CONCURRENTLY. 2 DAYS OF CLOSURE ONLY FOR PHASE 1.
2	RAMP 4 OUTSIDE THE OBJECT FREE AREA OF TAXIWAY A	RAMP 4	WORK IN PHASE 1, 2, 3 & 4 MAY BE DONE CONCURRENTLY.
3	RAMP 4 TAXILANES - CRACK SEAL	RAMP 4	WORK IN PHASE 1, 2, 3 & 4 MAY BE DONE CONCURRENTLY.
4	RAMP 4 TAXILANES - CRACK SEAL & ALTERNATE B1	RAMP 4	WORK IN PHASE 1, 2, 3 & 4 MAY BE DONE CONCURRENTLY.

SCHEDULE A  
SCHEDULE B  
ALTERNATE A1  
ALTERNATE B1



REVISIONS	BY	DATE
No.		

NEVADA COUNTY	NEVADA COUNTY AIRPORT
CALIFORNIA	RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4 TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD
	CONSTRUCTION SAFETY AND PHASING PLAN

DATE	2/7/2025
DRAWN	TS
CHECKED	DB
PROJECT No.	36.07
FILE	3607.02 CSPP
SCALE	1"=200'
SHEET No.	2 OF 17



GENERAL NOTES:

1.

CONSTRUCTION OF THIS PROJECT SHALL BE IN ACCORDANCE WITH THE CONTRACT AGREEMENT, THESE PLANS, TECHNICAL SPECIFICATIONS, SPECIAL PROVISIONS, INFORMATION FOR BIDDERS, AND ALL APPLICABLE FAA STANDARDS AND OTHER REFERENCED DOCUMENTS. CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE, AND LOCAL SAFETY AND ENVIRONMENTAL REGULATIONS. THE PROJECT IS SUBJECT TO INSPECTION OF THE OWNER AND THE RESIDENT PROJECT REPRESENTATIVE DESIGNATED BY THE OWNER, THE FEDERAL AVIATION ADMINISTRATION, AND ANY OTHER GOVERNING AGENCIES.
2.

THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY STATE AND LOCAL PERMITS PRIOR TO CONSTRUCTION OF THIS PROJECT.
3.

THE CONTRACTOR SHALL NOTIFY THE OWNER IN WRITING A MINIMUM OF 72 HOURS IN ADVANCE TO OBTAIN CLEARANCE FOR WORK.
4.

THE CONTRACTOR'S SUPERINTENDENT SHALL BE ON THE CONSTRUCTION SITE AT ALL TIMES DURING WORKING HOURS WHILE THIS PROJECT IS IN PROGRESS. SUPERINTENDENT SHALL BE CONTRACTOR'S DESIGNATED RESPONSIBLE REPRESENTATIVE AND SHALL BE AVAILABLE IN CASE OF EMERGENCIES ON A 24-HOUR DAILY BASIS. EMERGENCY PHONE NUMBERS SHALL BE PROVIDED AT THE START OF THE PROJECT.
5.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE CLEANLINESS, SAFETY, AND SECURITY OF THE WORK, STAGING AND STORAGE AREAS AT ALL TIMES.
6.

THESE PLANS SHOW ITEMS TO BE CONSTRUCTED UNDER THIS CONTRACT AND EXISTING FIELD CONDITIONS AT THE TIME THESE PLANS WERE PREPARED. THE EXISTING INFORMATION SHOWN ON THESE PLANS IS FROM THE BEST SOURCES AVAILABLE AT THE TIME OF COMPILATION. ACTUAL FIELD CONDITIONS, GRADES, LOCATIONS AND OTHER FEATURES MAY DIFFER FROM CONDITIONS INDICATED ON THESE DOCUMENTS. THE CONTRACTOR SHALL TAKE ALL MEASURES NECESSARY TO SATISFY HIMSELF THAT THE INFORMATION IS STILL CURRENT AT THE TIME OF CONSTRUCTION NOTICE TO PROCEED. CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER AND OWNER OF ANY DISCREPANCIES OR CHANGES ENCOUNTERED.
7.

ELECTRONIC FILES OF THESE PLANS MAY BE PROVIDED BY THE ENGINEER AS A CONVENIENCE TO THE CONTRACTOR. IF THERE ARE ANY DISCREPANCIES BETWEEN THE PLANS AND THE ELECTRONIC FILES, THE PLANS SHALL GOVERN. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IF ANY DISCREPANCIES ARE ENCOUNTERED. EXISTING GRADES SHOWN ARE LIMITED BY THE ACCURACY OF SURVEYING METHODS AND INTERPOLATION BETWEEN SURVEY POINTS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONSTRUCT THE PROJECT TO THE PRINTED PLANS AND SPECIFICATIONS AND IN ACCORDANCE WITH SOUND CONSTRUCTION PRACTICES.
8.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES, SERVICE LATERALS AND CONDUIT ("UTILITIES") IS BASED ON THE BEST AVAILABLE INFORMATION TO THE ENGINEER AND SHALL BE ASSUMED AS APPROXIMATE AND REQUIRING FIELD VERIFICATION. CONTRACTOR WILL BE RESPONSIBLE FOR LOCATING AND AVOIDING ALL UTILITIES AND FOR REPAIRING ALL DAMAGE THAT OCCURS TO DUE TO THE CONTRACTOR'S ACTIVITIES. CONTRACTOR SHALL CONTACT UNDERGROUND SERVICE ALERT PRIOR TO CONSTRUCTION, AND SHALL POTHOLE TO VERIFY LOCATION, DEPTH, AND SIZE OF UTILITIES WITHIN THE LIMITS OF CONSTRUCTION.
9.

EXERCISE EXTREME CARE WHEN USING ANY EQUIPMENT TO PREVENT CONTACT WITH ANY NEARBY POWER LINES AND POWER SOURCES. SAFE WORKING CLEARANCES SHALL CONFORM TO THE NATIONAL ELECTRIC CODE.
10.

THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND RPR ON THE PRECISE LOCATION AND LIMITS OF THE CONTRACTOR'S STAGING AND STORAGE AREA, AS WELL AS ANY SPECIAL REQUIREMENTS FOR FENCING, SECURITY OR ACCESS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE ALL UTILITIES AND HOOK-UPS NECESSARY FOR THE CONTRACTOR'S USE AND FOR ALL PROJECT FIELD OFFICES AS REQUIRED IN THE SPECIAL PROVISIONS. THE CONTRACTOR SHALL USE THE STAGING AND STORAGE AREA FOR SHOP, MATERIAL AND EQUIPMENT STORAGE, AND OTHER PROJECT-RELATED ACTIVITIES INCLUDING EMPLOYEE PARKING. ALL COSTS ASSOCIATED WITH PREPARATION AND CLEANUP OF THE STAGING AREA SHALL BE BORNE BY THE CONTRACTOR.
11.

ANY AND ALL REQUIRED UTILITIES FOR THE CONTRACTOR'S OPERATIONS SHALL BE ARRANGED FOR AND PAID FOR BY THE CONTRACTOR AND PAID DIRECTLY TO THE APPROPRIATE UTILITY. UTILITY ARRANGEMENTS SHALL BE SUBJECT TO THE APPROVAL OF THE OWNER AND RPR.
12.

THE CONTRACTOR SHALL NOT ENTER ONTO ANY AREA OUTSIDE OF THE CONSTRUCTION LIMITS, STAGING AREA, OR DESIGNATED HAUL ROUTES WITHOUT APPROVAL OF THE OWNER AND RPR.
13.

THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE TRAFFIC REGULATIONS CONCERNING THE USE OF STREETS AND ROADWAYS FOR HAULING. ANY DAMAGE DONE TO THE ROADWAYS DUE TO THE CONTRACTOR'S EQUIPMENT OR HAULING OPERATIONS SHALL BE REPAIRED TO THE OWNER'S SATISFACTION AT NO COST TO THE OWNER.
14.

NO MATERIAL SHALL BE WASTED OR STOCKPILED ON THE AIRPORT UNLESS APPROVED BY THE OWNER AND RPR. STOCKPILED MATERIAL SHALL MEET SWPPP REQUIREMENTS AND SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT AS A RESULT OF AIRCRAFT OPERATIONS OR WIND AND IN ACCORDANCE WITH FAA ADVISORY CIRCULARS.
15.

THE CONTRACTOR SHALL INVESTIGATE THE AVAILABILITY OF AN ADEQUATE SUPPLY OF SUITABLE WATER AND PROVIDE NECESSARY FACILITIES TO FURNISH WATER FOR USE DURING CONSTRUCTION, SOLELY AT HIS EXPENSE. CONTRACTOR SHALL NOT DRAW WATER FROM ANY FIRE HYDRANT FOR USE ON THE WORK WITHOUT FIRST OBTAINING WRITTEN APPROVAL FROM THE CONTROLLING FIRE DEPARTMENT OR UTILITY.
16.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL FIRE HYDRANTS AND BACKFLOW PREVENTERS AT ALL TIMES.
17.

THE CONTRACTOR SHALL SUBMIT A DRAWING SHOWING THE PROPOSED SITE LAYOUT OF ANY BATCH PLANTS. THE PLAN SHALL BE APPROVED BY THE OWNER AND RPR PRIOR TO STARTING ANY WORK.
18.

ANY WASTE, CONSTRUCTION DEBRIS, OR SOIL MUST BE DISPOSED OF PROPERLY. DISPOSAL OF MATERIAL OFF-SITE SHALL BE DONE IN A LAWFUL MANNER AND AT A SITE HAVING CURRENT APPROVAL TO ACCEPT SOLID WASTE. DISPOSAL SITE AND PROCEDURES MUST BE IDENTIFIED BY THE CONTRACTOR AND SUBMITTED TO THE OWNER AND RPR FOR APPROVAL PRIOR TO USE.
19.

CONTRACTOR SHALL HAVE SPILL KITS AVAILABLE IN WORK AREAS AND SHALL CONTAIN ALL SPILLS IMMEDIATELY AND SHALL NOTIFY RPR. AT ANY SIGN OF CONTAMINATED SOIL, THE CONTRACTOR SHALL NOTIFY RPR AND OWNER FOR ASSESSMENT OF APPROPRIATE REMEDIATION.
20.

ANY PRODUCTS IN 5 GALLON CONTAINERS OR LARGER MUST HAVE SECONDARY CONTAINMENT AND KEPT AWAY FROM STORM DRAINS. ENSURE ALL STORM DRAINS ARE PROTECTED IN CONSTRUCTION AND STAGING AREAS.
21.

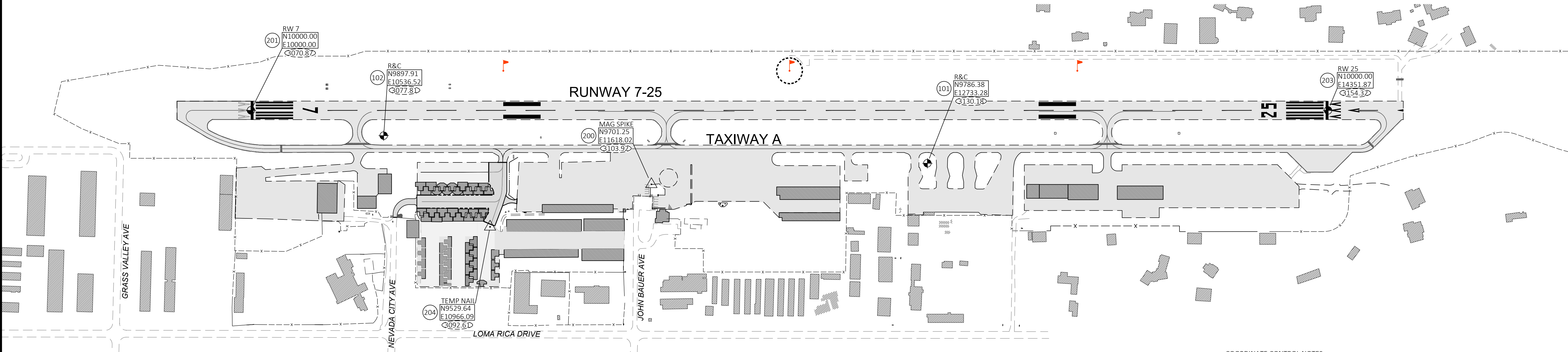
ANY SOLVENT USED TO CLEAN TOOLS, EQUIPMENT, OR SPILLS MAY BE CONSIDERED A HAZARDOUS WASTE AND MUST BE PROPERLY MANAGED. NO SOLVENTS, CLEANING BY-PRODUCTS, WASTE, REFUSE, OR LEFTOVER PAINT MAY BE DISPOSED OF OR DISCHARGED INTO STORM DRAINS, DRYWELLS, OR ANY GROUND SURFACE, OR OTHERWISE BE PERMITTED TO REMAIN ON AIRPORT PROPERTY. ALL SUCH MATERIAL SHALL BE REMOVED OFF-SITE BY CONTRACTOR IN ACCORDANCE WITH APPLICABLE LAWS AND REGULATIONS.
22.

THE CONTRACTOR SHALL CONDUCT THE FINAL CLEANING OF AFFECTED AIRPORT PAVEMENTS PRIOR TO REOPENING THE PAVEMENTS TO AIR TRAFFIC. CONTRACTOR TO PROTECT ALL EXISTING UTILITY VAULTS AND LIDS DURING CONSTRUCTION. CONTRACTOR SHALL ENSURE THAT ALL VAULT LIDS ARE OPERATIONAL FOLLOWING COMPLETION OF CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR CONTINUOUS DAILY CLEAN-UP OF THE WORK AREA. NO WIRE OR METAL BRISTLES ARE ALLOWED ON AIRFIELD PAVEMENTS.
23.

THE CONTRACTOR SHALL COMPLETE CLEANUP AND RESTORATION OF THE ENTIRE PROJECT AREA, INCLUDING STAGING AND STORAGE AREAS AND BATCH PLANTS PRIOR TO PROJECT FINAL ACCEPTANCE.

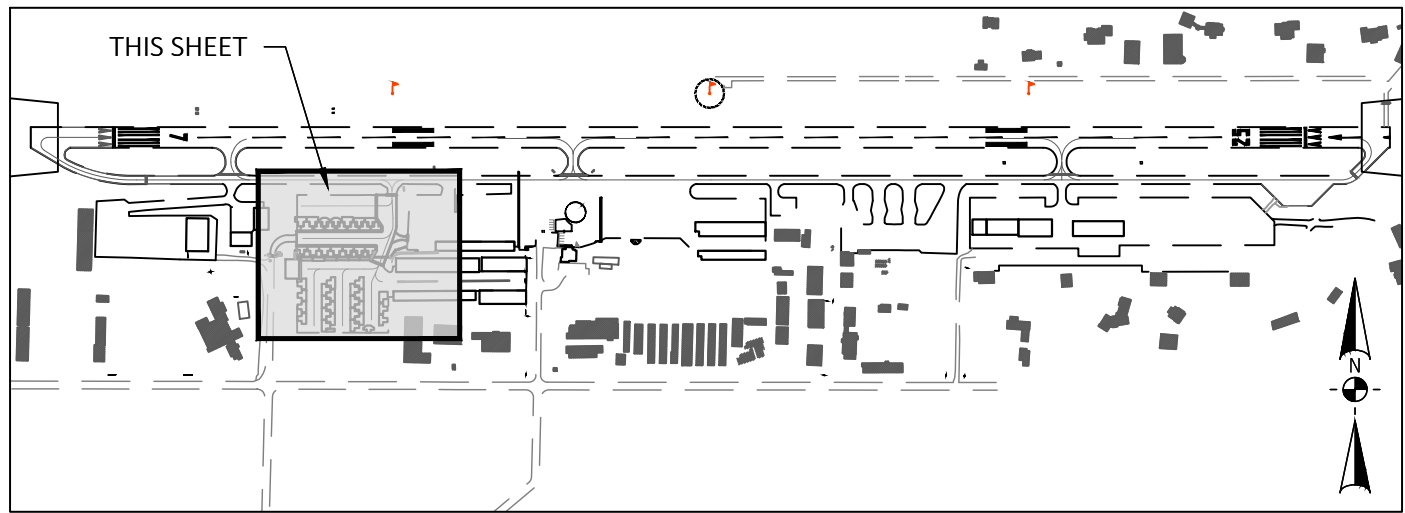
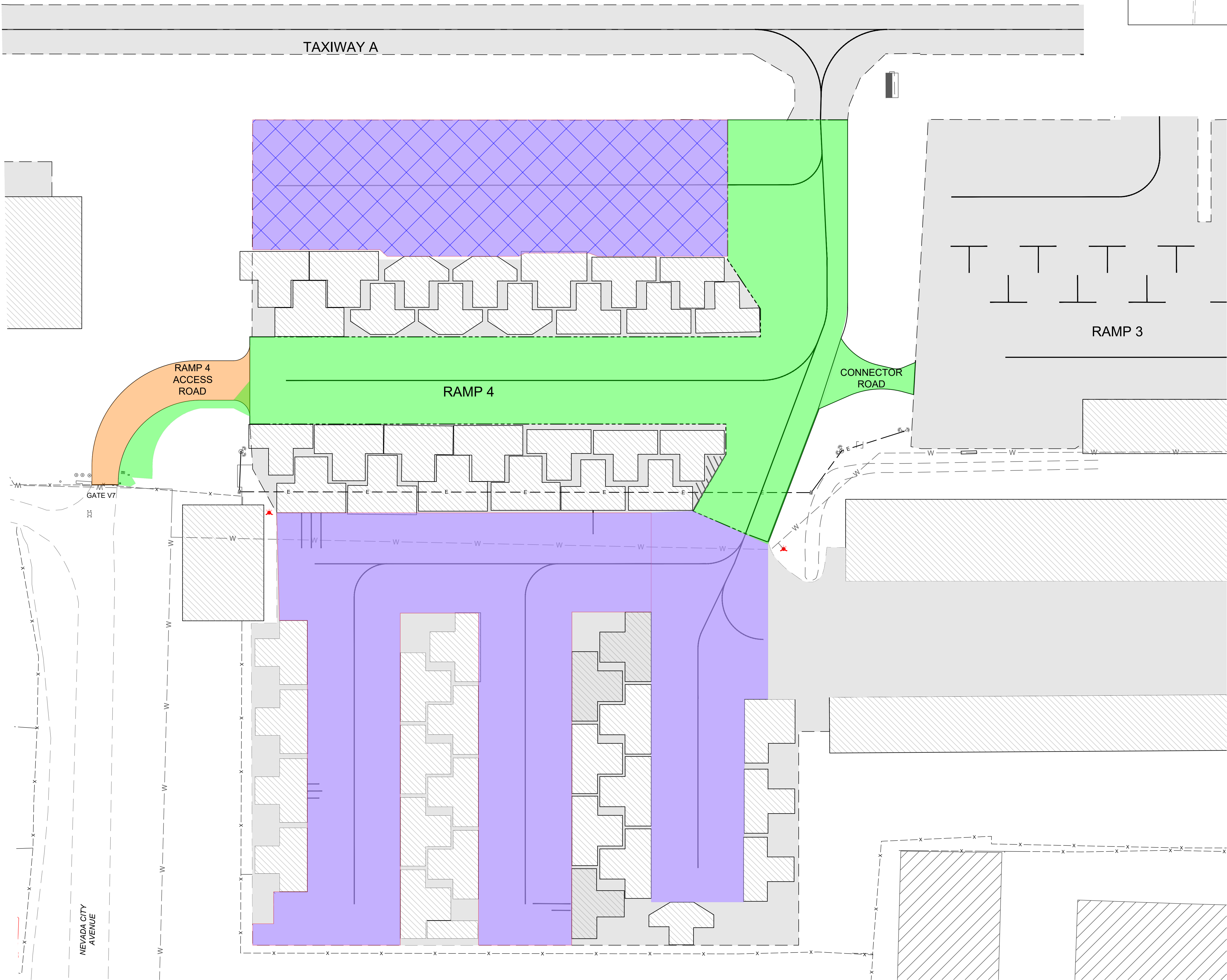
COMMON ABBREVIATIONS:

AB	AGGREGATE BASE	MIN	MINIMUM
ABND	ABANDONED	MON	MONUMENT
AC	ASPHALT CONCRETE	N	NORTH
ALT	ALTERNATE	NE	NORTHEAST
APPROX	APPROXIMATE	NIC	NOT IN CONTRACT
ASB	AGGREGATE SUBBASE	No., #	NUMBER
ATCT	AIR TRAFFIC CONTROL TOWER	NTAM	NOTICE TO AIR MISSIONS
AWG	AMERICAN WIRE GAUGE	NTS	NOT TO SCALE
BN DY	BOUNDARY	NW	NORTHWEST
BLD	BUILDING	OC	ON CENTER
BM	BENCH MARK	OD	OUTSIDE DIAMETER
CBR	CALIFORNIA BEARING RATIO	PAPI	PRECISION APPROACH PATH INDICATOR
CL OR C	CENTERLINE	PB	PULL BOX
COMM	COMMUNICATION	PC	POINT OF CURVATURE
COORD	COORDINATE	PCC	PERMEABLE CONCRETE BASE COURSE
CTAF	COMMON TRAFFIC ADVISORY FREQUENCY	PCC	PORTLAND CEMENT CONCRETE
CSPP	CONSTRUCTION SAFETY & PHASING PLAN	PI	POINT OF INTERSECTION
CU YD OR CY	CUBIC YARD	PL	PROPERTY LINE
DIA	DIAMETER	PT	POINT OF TANGENCY
DI	DROP INLET	PVI	POINT OF VERTICAL INTERSECTION
DIP	DUCTILE IRON PIPE	PWR	POWER
E	EAST	R	RADIUS
EA	EACH	R	REINFORCED CONCRETE PIPE
EG	EXISTING GRADE (OR GROUND)	RPR	RESIDENT PROJECT REPRESENTATIVE
EX, EXIST	EXISTING	RT	RIGHT
EXC	EXCAVATION	RW	RUNWAY
FAA	FEDERAL AVIATION ADMINISTRATION	S	SOUTH, OR SLOPE
FBO	FIXED BASED OPERATOR	SCH	SCHEDULE
FG	FINISHED GRADE (OR GROUND)	SD	STORM DRAIN
FH	FIRE HYDRANT	SDR	STANDARD DIMENSION RATIO
FL	FLOW LINE	SE	SOUTHEAST
FT	FEET	SPEC	SPECIFICATIONS
GAL	GALLON	SS	SANITARY SEWER
GALV	GALVANIZED	SQ YD, SY	SQUARE YARD
GB	GRADE BREAK	SW	SOUTHWEST
GND	GROUND	SWPPP	STORM WATER POLLUTION PREVENTION PLAN
HGR	HANGAR	TD	TOP OF DUCT
HH	HANDHOLE	TL	TAXILANE
HORZ	HORIZONTAL	TW	TAXIWAY
ID	INSIDE DIAMETER	TYP	TYPICAL
INV	INVERT	UG	UNDERGROUND
kv	KILOVOLT	VAR	VARIES (OR VARIABLE)
kVA	KILOVOLT AMPERE	VOL	VOLUME
L	LENGTH	W/	WITH
LF	LINEAR FEET	W	WEST, OR WIDTH, OR WATER
LT	LEFT	WM	WATER METER
MAX	MAXIMUM	WV	WATER VALVE
MH	MANHOLE		





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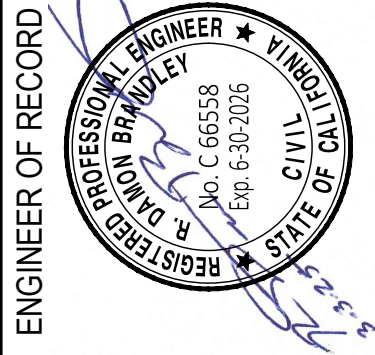


LOCATION MAP

- LEGEND
- EXISTING EDGE OF PAVEMENT
  - SCHEDULE A
  - ALTERNATE A1
  - SCHEDULE B
  - ALTERNATE B1

- NOTES:
- SCHEDULE A CONSISTS OF THE RECONSTRUCTION OF RAMP 4 TAXILANES AND CONNECTOR TO RAMP 3 AS DEPICTED.
  - ALTERNATE A1 CONSISTS OF THE RECONSTRUCTION OF THE RAMP 4 ACCESS ROAD.
  - SCHEDULE B CONSISTS OF THE CRACK SEAL OF RAMP 4 TAXILANES AS DEPICTED.
  - ALTERNATE B1 IS THE INCREASE IN COST TO USE MAXWELL PRODUCTS GAP-PATCH 550 MATERIAL INSTEAD OF ASTM D8260 TYPE 1 MASTIC FOR ALL TYPE S CRACK SEAL AREAS.

SCHEDULE A  
SCHEDULE B  
ALTERNATE A1 & B1

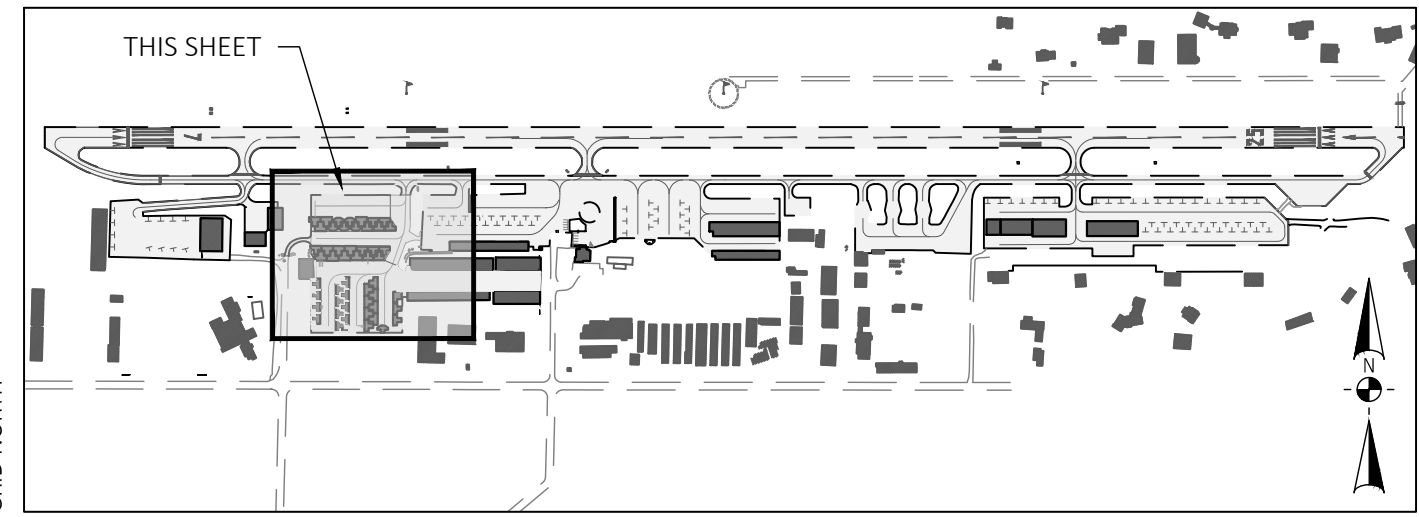
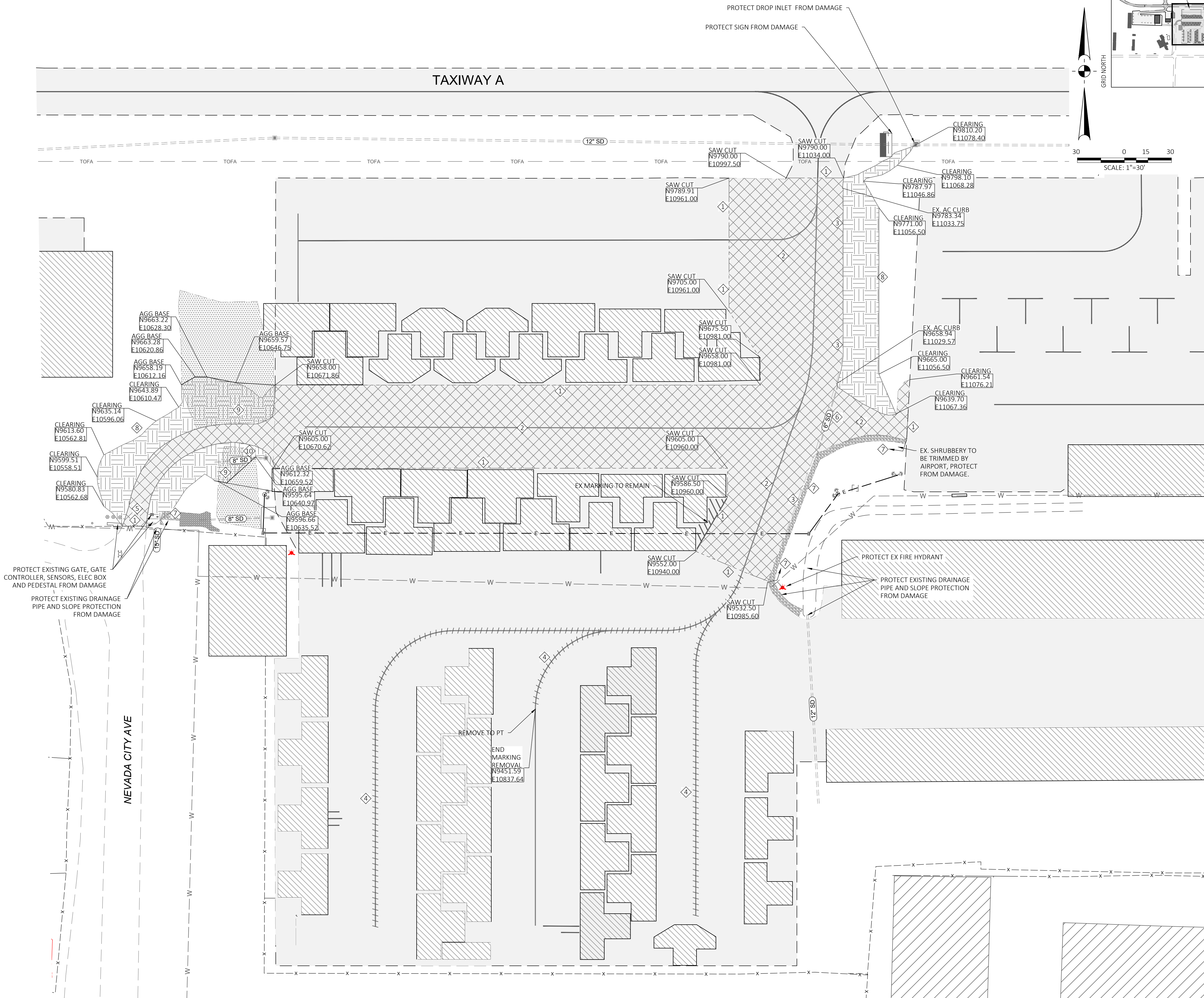


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NEVADA COUNTY AIRPORT  
CALIFORNIA  
RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD  
BID SCHEDULE KEY MAP

DATE	2/7/2025
DRAWN	TS
CHECKED	DB
PROJECT No.	36.07
FILE	3607.07.Schedule
SCALE	1"=30'
SHEET No.	4 OF 17





LOCATION MAP

LEGEND

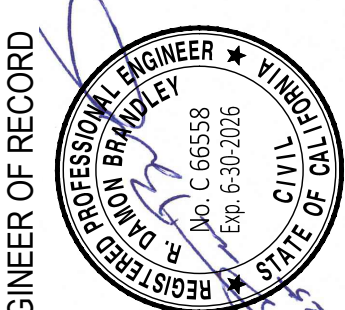
- N9738.46  
E13727.35  
22.00  
22.00
- AIRPORT GRID COORDINATE
- FINISHED GRADE ELEVATION
- EXISTING GRADE ELEVATION
- EXISTING PAVEMENT
- PULVERIZE, EXCAVATE, AND STOCKPILE
- EXISTING PAVEMENT
- EXISTING BUILDING
- CLEAR AND GRUB AREA
- MATCH EXISTING GRADE- SAW CUT
- EXISTING FENCE
- EXISTING ELECTRICAL DUCT
- EXISTING WATER LINE
- EXISTING STORM DRAIN
- EXISTING DROP INLET

- SAW CUT MATCH LINE
- FULL DEPTH RECLAMATION - PULVERIZE EXISTING AC AND EXISTING AGGREGATE BASE TO BE STOCKPILED FOR REUSE AS FDR AGGREGATE SUBBASE - REMOVE AND DISPOSE OF EXISTING CONCRETE TIE DOWN ANCHORS PRIOR TO PULVERIZING OPERATIONS.
- REMOVE EXISTING AC CURB
- REMOVE EXISTING MARKING PRIOR TO CRACK SEALING. AREAS OF MARKING REMOVAL SHALL BE SEALED WITH A SS1H SEAL COAT.
- REMOVE EXISTING GATE DETECTOR LOOP, SEE DETAIL SHEET 7. RECORD EXISTING LOCATION AND SIZE PRIOR TO REMOVAL.
- REMOVE EXISTING 6" DRAINAGE PIPE.
- REMOVE, STOCKPILE AND REUSE EXISTING COBBLES
- LIMITS OF CLEARING AND GRUBBING. SEE GRADING PLAN SHEETS 6 - 9. AREAS TO BE RESEED
- REMOVE EXISTING AGGREGATE BASE IN LIMITS OF NEW GRADING, STOCKPILE AND REUSE IN ORIGINAL LOCATION.
- REMOVE EXISTING 8" DRAINAGE PIPE AND EXISTING INLET STRUCTURE.

DEMOLITION ITEM PAYMENT NOTES:

- ALL DEMOLITION ITEMS SHALL BE HAULED OFF AND DISPOSED OF OFF SITE UNLESS THE ITEM IS NOTED TO BE REUSED OR REPLACED ON-SITE.
- DEMOLITION OF ITEMS FOR THE REMOVAL OF AC CURB, PIPE REMOVAL, AND DROP INLET STRUCTURE REMOVAL WILL BE CONSIDERED INCIDENTAL TO THE CLEARING AND GRUBBING BID ITEM. NO SEPARATE PAYMENT WILL BE MADE FOR DEMOLITION AND REMOVAL OF THESE ITEMS.
- THERE ARE EXISTING CONCRETE ENCASED TIE DOWN ANCHORS IN THE EXISTING ASPHALT PAVEMENT SECTION. TIE DOWN ANCHORS IN AREAS TO BE RECONSTRUCTED SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. REMOVAL OF TIE DOWNS SHALL BE CONSIDERED INCIDENTAL TO THE FDR PULVERIZING. USE CAUTION WHEN PULVERIZING SO THAT THESE TIE DOWN ANCHORS DON'T DAMAGE PULVERIZING EQUIPMENT.
- EXISTING AGGREGATE BASE COURSE THAT IS CALLED TO BE REMOVED, AND REPLACED AT THE END OF THE PROJECT WILL BE PAID BY THE CUBIC YARD IN ITS ORIGINAL POSITION UNDER THE UNCLASSIFIED EXCAVATION BID ITEM.
- EXISTING COBBLES OR RIP RAP THAT IS CALLED TO BE REMOVED AND REPLACED AT THE END OF THE PROJECT WILL BE PAID BY THE SQUARE YARD IN THE FINAL IN PLACE POSITION UNDER THE RIP RAP BID ITEM. EXISTING RIP RAP AREAS TYPICALLY HAVE A SINGLE LAYER OF RIP RAP COBBLE ON TOP OF FILTER FABRIC. NEW RIP RAP SHALL BE INSTALLED PER THE DETAILS ON THE PLANS.
- THE RIP RAP BID ITEM IS PAID BY THE SQUARE YARD OF RIP RAP IN ITS FINAL POSITION, REGARDLESS OF WHETHER THE RIP RAP WAS REMOVED AND REUSED ON THE PROJECT OR IMPORTED. THIS ITEM WILL BE USED FOR ALL RIP RAP OR COBBLES THAT ARE NOTED TO BE REMOVED, STOCKPILED, AND REPLACED AT THE END OF THE PROJECT. THIS ITEM WILL ALSO BE UTILIZED FOR NEW RIP RAP THAT IS IMPORTED, HAULED, AND PLACED BY THE CONTRACTOR.
- THE MARKING REMOVAL BID ITEM SHALL INCLUDE THE REMOVAL OF THE EXISTING MARKING BY WATERBLASTING OR MICROGRINDING, SO THAT THE EXISTING PAVEMENT IS NOT DAMAGE. A SS1H SEAL COAT SHALL BE APPLIED TO MARKING REMOVAL AREAS. NO SEPARATE PAYMENT WILL BE MADE FOR THIS SEAL COAT AS IT WILL BE INCIDENTAL TO THE MARKING REMOVAL BID ITEM.
- SEE TYPICAL SECTION NOTES ON SHEET 9 FOR DETAILS ON FULL DEPTH RECLAMATION BID ITEMS AND PAVEMENT SECTION BID ITEMS.
- REPLACEMENT OF TWO EXISTING GATE LOOPS SHALL BE AS SHOWN ON SHEET 7. THE LUMP SUM BID ITEM FOR GATE LOOP REPLACEMENT SHALL INCLUDE THE REMOVAL OF THE EXISTING GATE LOOPS AND INSTALLATION OF THE NEW GATE LOOPS ONCE ACCESS ROAD IS COMPLETED. CONTRACTOR SHALL DETERMINE HOW THE EXISTING GATE LOOPS ARE CONNECTED TO THE GATE OPERATOR AND ENSURE THAT THE NEW GATE LOOPS WILL BE COMPATIBLE AND PROPERLY FUNCTION WITH THE EXISTING GATE OPERATOR AND CONTROLLER. NEW GATE LOOPS SHALL BE LOCATED IN THE SAME LOCATION AS THE EXISTING GATE LOOPS.

SCHEDULE A  
ALTERNATE A1  
SCHEDULE B



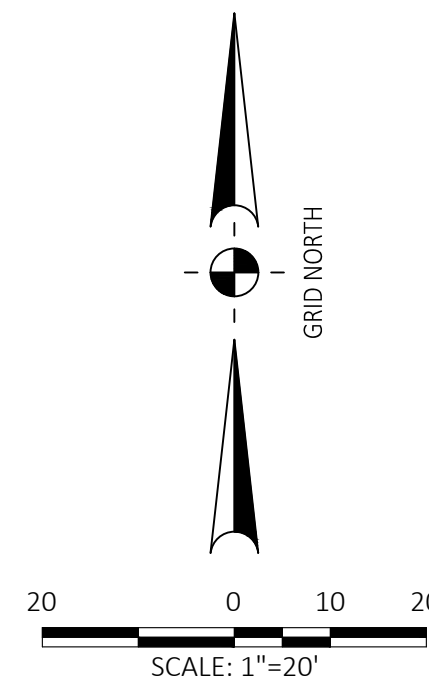
REVISIONS	BY	DATE
No.		

NEVADA COUNTY AIRPORT  
RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD  
DEMOLITION PLAN

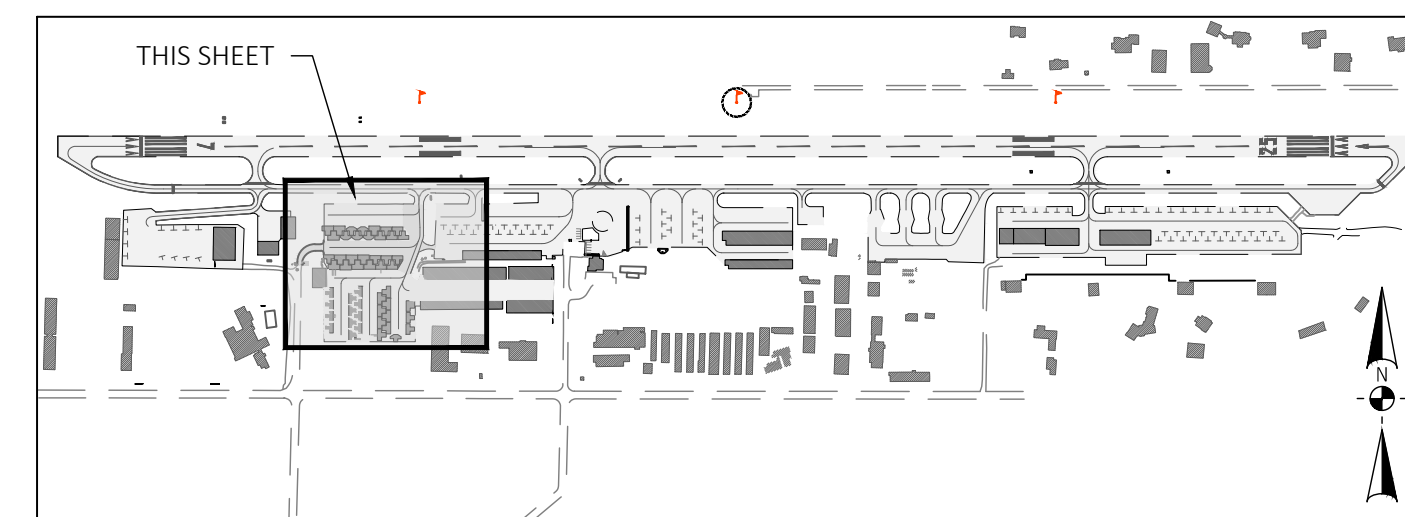
DATE	2/7/2025
DRAWN	KDC
CHECKED	DB
PROJECT No.	36.07
FILE	3607.04.Demo
SCALE	1"=30'
SHEET No.	5 OF 17



	AIRPORT GRID COORDINATE
	FINISHED GRADE ELEVATION
	EXISTING GRADE ELEVATION
	EXISTING PAVEMENT
	NEW PAVEMENT SECTION
	EXISTING BUILDING
	CENTERLINE
	GRADE BREAK
	FINISHED GRADE CONTOUR
	EXISTING GRADE CONTOUR
	MATCH EXISTING GRADE
	EXISTING FENCE
	EXISTING ELECTRICAL DUCT
	EXISTING WATER LINE
	NEW STORM DRAIN
	EXISTING STORM DRAIN
	EXISTING DROP INLET

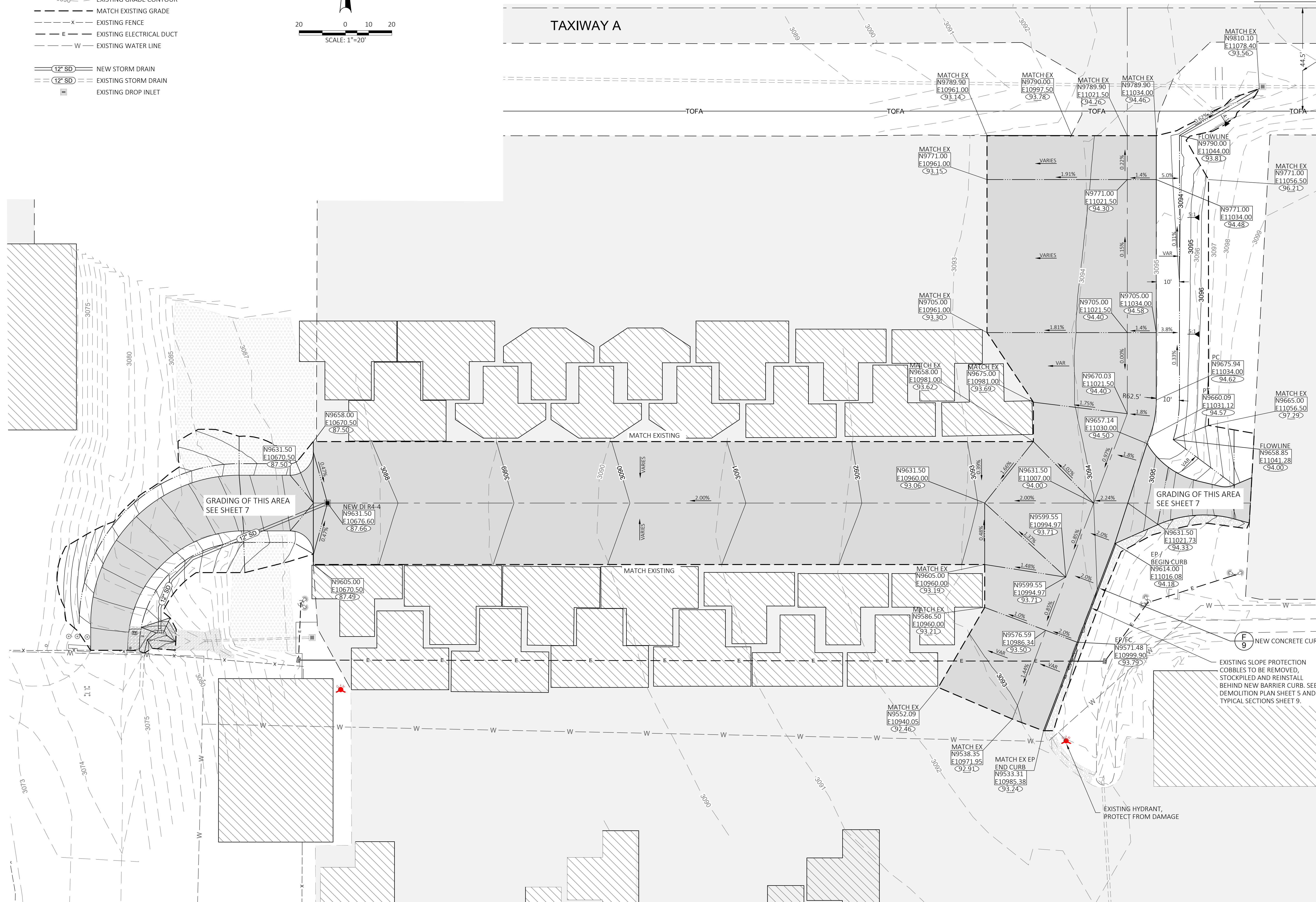


1. SEE CONSTRUCTION SAFETY AND PHASING PLAN SHEET 2 FOR WORK AREA BARRICADE LAYOUT AND PHASING RESTRICTIONS.
2. FOR EXISTING PAVEMENT DEMOLITION LIMITS SEE SHEET 5.
3. SEE SHEET 8 FOR LIMITS OF PAVEMENT SECTIONS 1 & 2.
4. SEE SHEET 9 FOR TYPICAL PAVEMENT SECTION AND SLOPE PROTECTION DETAILS .
5. SEE SHEET 10 & 11 FOR NEW DRAINAGE PIPE AND DROP INLET CONSTRUCTION AND DETAILS.
6. CONTRACTOR SHALL POTHOLE AND LOCATE EXISTING UTILITIES LOCATED WITHIN THE NEW PAVEMENT SECTION. SEE SHEET 11 FOR REQUIREMENTS FOR THE CONCRETE CAPPING OF EXISTING UTILITIES.

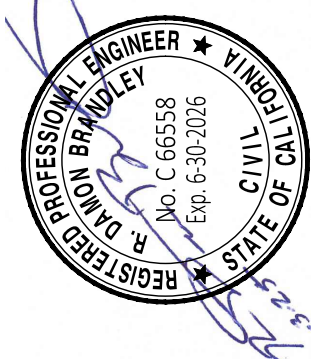


LOCATION MAP

CONSTRUCTION WITHIN THE TAXIWAY OBJECT FREE AREA WILL BE RESTRICTED TO THE PHASING SET IN THE CONSTRUCTION SAFETY AND PHASING PLAN. SEE SHEET 2



ENGINEER OF RECORD

[illegible]

NEVADA COUNTY AIRPORT

NEVADA COUNTY

RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4

TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD

TAXILANE GRADING PLAN

CALIFORNIA

DATE	2/7/2023
DRAWN	KDC
CHECKED	DE
PROJECT No.	36.07
FILE	3607.06.Grade
SCALE	1"=20'
SHEET No.	





**NEVADA COUNTY AIRPORT**  
NEVADA COUNTY CALIFORNIA  
**RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD  
ACCESS & CONNECTOR ROAD GRADING  
PLAN**

DATE	2/7/2025
DRAWN	KDC
CHECKED	DB
PROJECT No.	36.07
FILE	3607.06.RdGrade
SCALE	1"=10'
SHEET No.	



LEGEND

- N9738.46  
E13727.35

22.00

22.00

AIRPORT GRID COORDINATE  
FINISHED GRADE ELEVATION  
EXISTING GRADE ELEVATION
- EXISTING PAVEMENT  
EXISTING BUILDING
- CENTERLINE
- 3090

EXISTING GRADE CONTOUR
- MATCH EXISTING GRADE
- X

EXISTING FENCE
- E

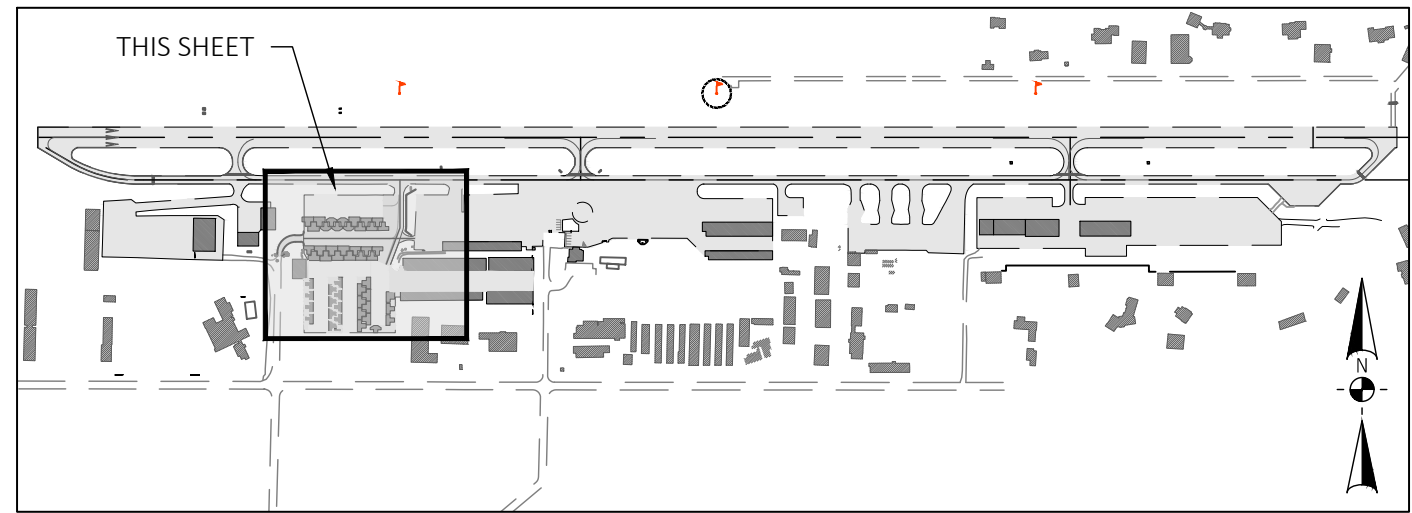
EXISTING ELECTRICAL DUCT
- W

EXISTING WATER LINE
- 18" SD

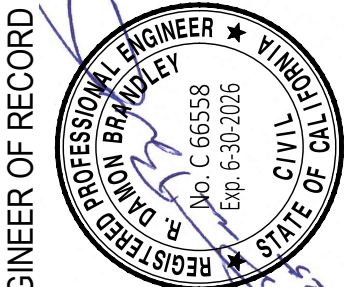
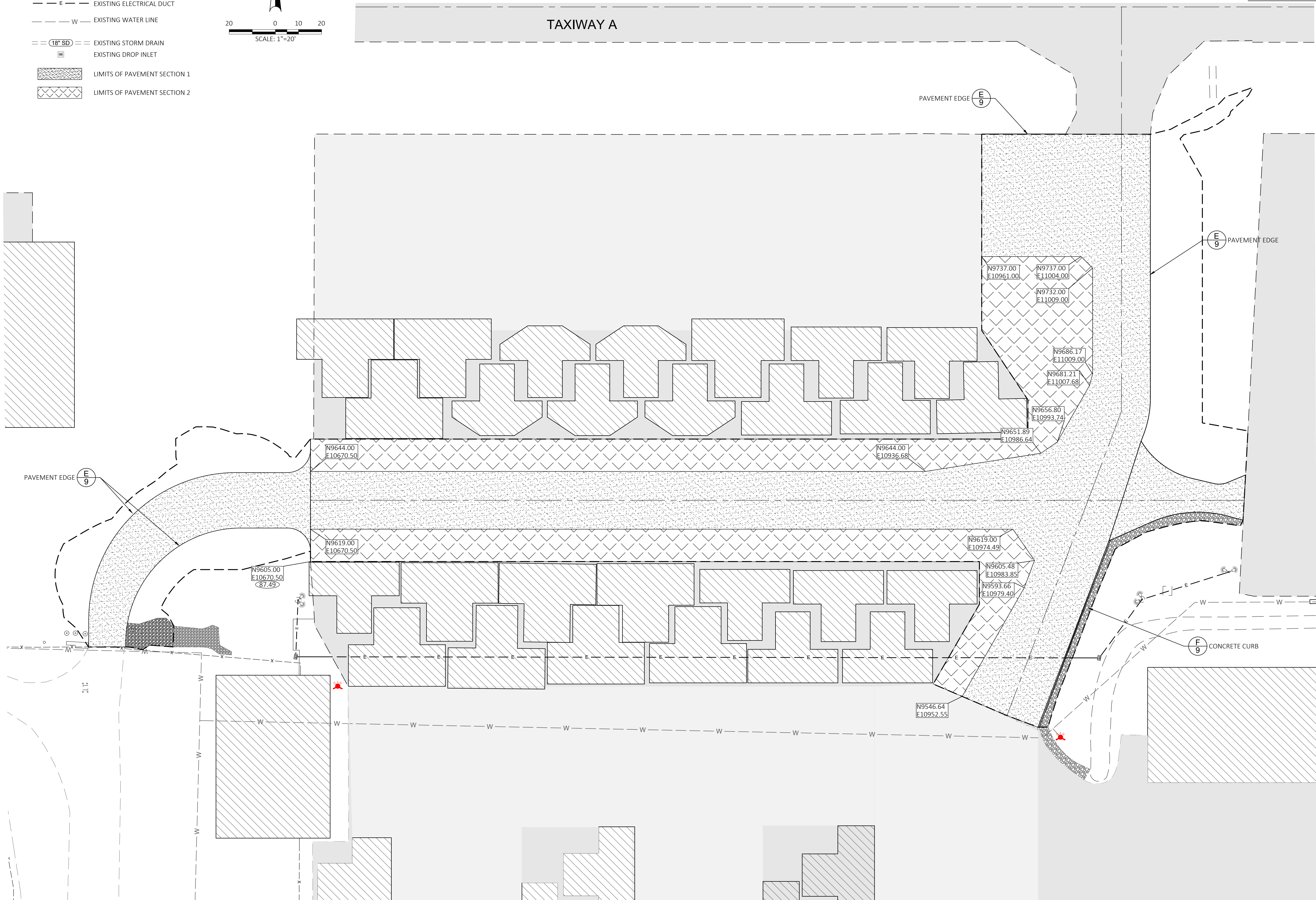
EXISTING STORM DRAIN
- EXISTING DROP INLET
- LIMITS OF PAVEMENT SECTION 1
- LIMITS OF PAVEMENT SECTION 2

NOTES:

- SEE CONSTRUCTION SAFETY AND PHASING PLAN SHEET 2 FOR WORK AREA BARRICADE LAYOUT AND PHASING RESTRICTIONS.
- FOR EXISTING PAVEMENT DEMOLITION LIMITS SEE SHEET 5.
- SEE SHEET 6 TO 7 GRADING PLANS.
- COORDINATES ON THIS SHEET SHOW THE PAVEMENT SECTION LIMITS OF THE FDR PLACED AS AGGREGATE SUBBASE. AGGREGATE SUBBASE EXTENDS 2 FEET BEYOND EDGE OF PAVEMENT AND BACK OF BARRIER CURB.
- SEE SHEET 9 FOR TYPICAL PAVEMENT SECTION, PAVEMENT EDGE, BARRIER CURB AND SLOPE PROTECTION DETAILS.



LOCATION MAP



No.	REVISIONS	BY	DATE

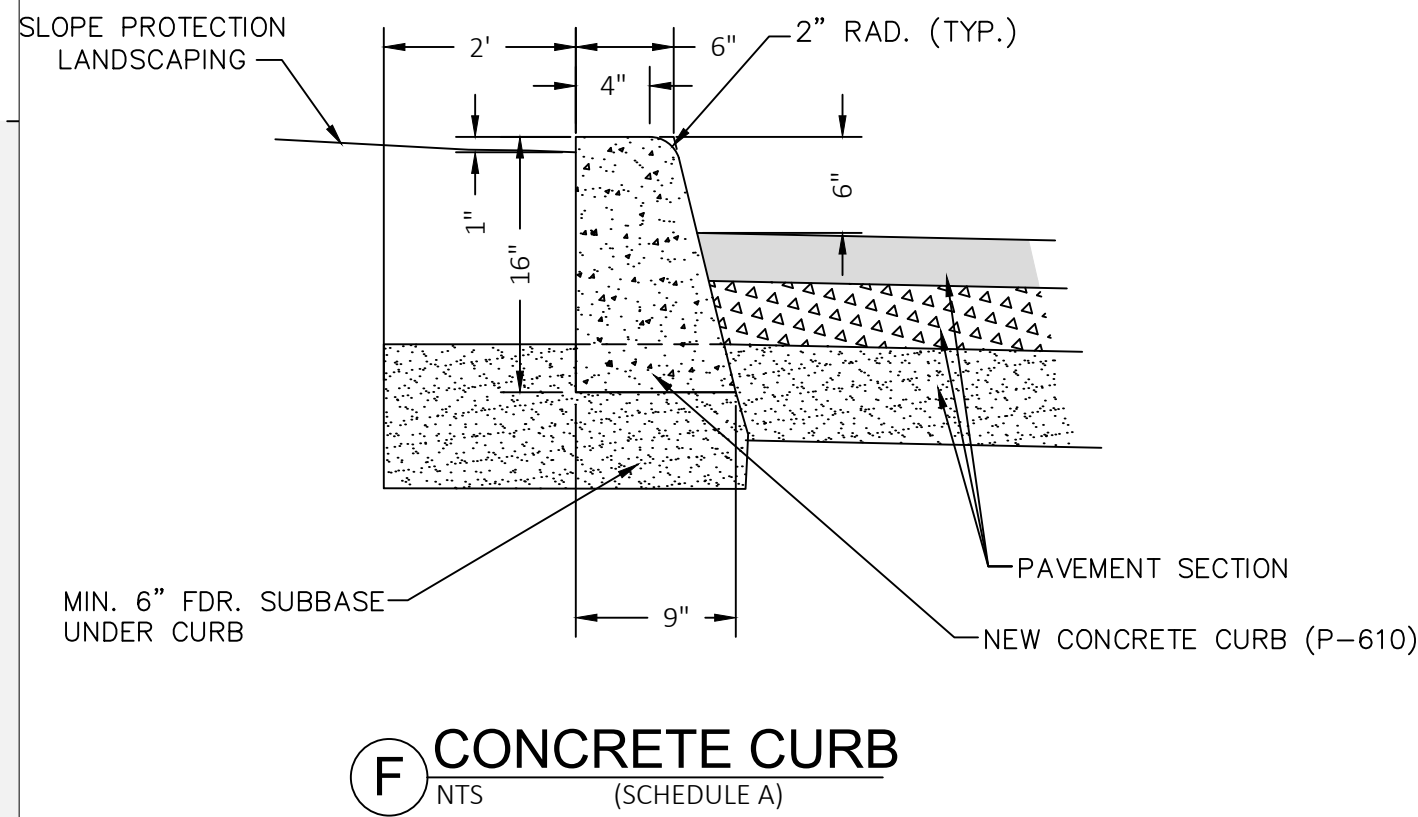
NEVADA COUNTY AIRPORT  
CALIFORNIA  
RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD  
PAVEMENT SECTION ZONE PLAN

DATE	2/7/2025
DRAWN	KDC
CHECKED	DB
PROJECT No.	36.07
FILE	3607.08.Eligibility
SCALE	1"=20'
SHEET No.	8 OF 17

SCHEDULE A  
ALTERNATE A1



TAXIWAY A



GENERAL NOTES FOR CONCRETE SCORING, EXPANSION, AND CONCRETE CURB REQUIREMENTS

- PLACE CONTROL JOINTS IN ACCORDANCE WITH ACI 224.3R-95, ACI 325.12R-02
- SUBGRADE SHALL BE SCARIFIED A MIN. OF 12 INCHES, MOISTURE CONDITIONED, AND COMPACTED TO A MIN. OF 95% RELATIVE COMPACTION.
- TOP AND FACE OF CURB SHALL BE TROWEL SMOOTH AND FINISHED WITH A FINE BRUSH.
- CONFORM TO AMERICAN PUBLIC WORKS ASSOCIATION (SOUTHERN CALIFORNIA CHAPTER) - STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION CURRENT VERSION.
- CONCRETE SCORING AND EXPANSION REQUIREMENTS SHALL BE IN ACCORDANCE WITH ACI 224.3R-95.
- EXPANSION JOINTS ARE ONLY NECESSARY AT RELATIVELY FIXED STRUCTURES SUCH AS A LIGHT POLE FOOTING AND DROP INLET BOXES.
- CONTRACTION JOINTS SHALL BE SPACED AT 10' O.C. MAXIMUM.

PAVEMENT SECTION NOTES:

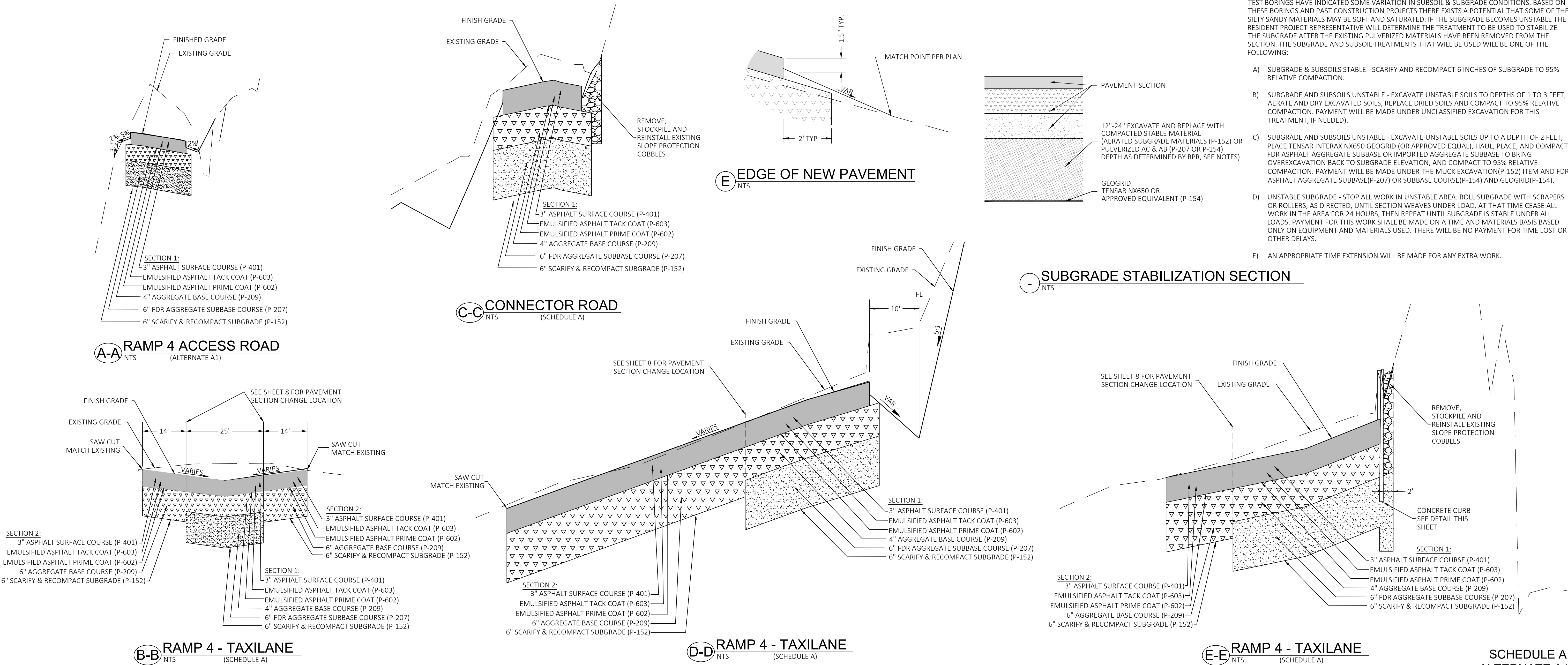
- EXISTING PAVEMENT SHALL BE PULVERIZED TO A TOTAL DEPTH OF 6 INCHES. THE PULVERIZING OPERATIONS SHALL BE SUCH THAT IT WILL THOROUGHLY MIX THE PULVERIZED PAVEMENT SECTION MATERIALS PER THE REQUIREMENTS OF P-207. CONTRACTOR SHALL ENSURE THAT UNDERLYING SUBGRADE MATERIALS ARE NOT MIXED INTO THESE MATERIALS. SEE SHEETS 16-17 FOR SOIL BORING LOGS OF EXISTING SECTION THICKNESS. CONTRACTOR SHALL POthOLE DURING PULVERIZATION TO ENSURE NO CONTAMINATION IS OCCURRING.
- PULVERIZING OF EXISTING AC WITH EXISTING AB, AS FDR AGGREGATE SUBBASE COURSE WILL BE PAID UNDER A SINGLE BID ITEM IN SPECIFICATION SECTION P-207 AND AS DESCRIBED BELOW:
  - "IN-PLACE FULL DEPTH RECYCLED (FDR) ASPHALT AGGREGATE SUBBASE COURSE - PULVERIZE, MIX, EXCAVATE, STOCKPILE, HAUL, PLACE, AND COMPACT" WILL BE PAID BY THE CUBIC YARD AS MEASURED IN ITS FINAL COMPACTED POSITION IN AREAS DESIGNATED ON THE PLANS AS "FDR AGGREGATE SUBBASE COURSE". THIS ITEM WILL INCLUDE PULVERIZING AND MIXING THE EXISTING PAVEMENT SECTION MATERIALS (AC WITH EXISTING AB), EXCAVATION AND TEMPORARY STOCKPILING OF THE PULVERIZED MATERIALS, AND THE HAULING, PLACING, AND COMPACTING OF THE FDR MATERIALS IN THEIR FINAL POSITION AS EITHER AGGREGATE SUBBASE COURSE. THIS PAY ITEM WILL ALSO BE USED TO PAY FOR ANY FDR MATERIALS USED TO FILL "MUCK EXCAVATION" AREAS OF UNSTABLE SUBGRADE.
- ALL AC SAW CUTTING SHALL BE CONSIDERED INCIDENTAL TO BID ITEMS UNDER P-207. NO SEPARATE PAYMENT SHALL BE MADE FOR AC SAW CUTTING.
- IMPORTED "AGGREGATE SUBBASE COURSE" AS SPECIFIED IN SECTION P-154 SHALL BE USED ONLY AFTER ALL P-207 MATERIALS HAVE BEEN USED.
- SEE SHEET 8 FOR LIMITS OF PAVEMENT SECTIONS 1 AND 2.
- ANY EXCAVATION OF UNSTABLE SUBGRADE MATERIALS WILL BE PAID UNDER "MUCK EXCAVATION" UNDER SPECIFICATION P-152. SEE SUBGRADE STABILIZATION DETAIL ON SHEET 9. ALL EXCAVATION OF SUBGRADE SOILS WILL BE PAID UNDER "UNCLASSIFIED EXCAVATION". IN NO CASE SHALL ANY FDR AGGREGATE SUBBASE COURSE MATERIALS BE PAID UNDER UNCLASSIFIED EXCAVATION. ALL WORK FOR P-207 MATERIALS WILL ONLY BE PAID UNDER THE RESPECTIVE BID ITEM FOR P-207.
- ALL EXCESS UNCLASSIFIED EXCAVATION AND MUCK EXCAVATION MATERIALS NOT USED AS EMBANKMENT SHALL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
- PRIME AND TACK COAT MATERIALS SHALL MEET ASTM D977, D2397, OR D2027. "TOPEIN" OIL MAY NOT BE USED FOR PRIME OR TACK COAT.
- ASPHALT SURFACE COURSE TO BE PLACED IN TWO LIFTS, EXCEPT THAT THE ASPHALT ON THE ACCESS AND CONNECTOR ROAD MAY BE PLACED IN A SINGLE 3" LIFT. CONTRACTOR IS REQUIRED TO MEET ALL REQUIREMENTS OF P-401 INCLUDING SMOOTHNESS AND GRADE TOLERANCES. ALL GRADE CONTROL FOR ASPHALT SURFACE COURSE SHALL BE DONE BY STRING LINE UNLESS A SUPERIOR METHOD IS PROPOSED BY THE CONTRACTOR AND APPROVED BY THE RPR.
- ALL ASPHALT SHALL BE PLACED IN CONTINUOUS LANES WITHOUT ANY TRANSVERSE JOINTS. THE ONLY CASES IN WHICH TRANSVERSE JOINTS MAY BE ALLOWED DUE TO UNFORESEEN MECHANICAL BREAKDOWNS.
- AT THE CONCLUSION OF ALL WORK, CONTRACTOR SHALL SEED CONTRACTOR'S STORAGE, STAGING AND TEMPORARY STOCKPILE AREA AND ALL DISTURBED GRADING OR EMBANKMENT AREAS. LIMITS OF SEEDING SHALL BE VERIFIED BY RPR. HYDROSEEDING SHALL BE A REQUIRED BMP TO BE INCLUDED IN THE SWPPP. NO ADDITIONAL PAYMENT FOR SURFACE PREPARATION OR SEEDING OF DISTURBED AREAS SHALL BE MADE, COST SHALL BE INCLUDED IN THE COST OF PREPARING AND IMPLEMENTING THE SWPPP. SEED MIX SHALL BE AS SHOWN ON SHEET 2. HYDROSEEDING MUST PRODUCE ADEQUATE GROWTH TO SATISFY THE STATE WATER BOARD SUCH THAT THE SWPPP CAN BE CLOSED OUT AFTER THE PROJECT IS COMPLETE.

SUBGRADE STABILIZATION NOTES:

TEST BORINGS HAVE INDICATED SOME VARIATION IN SUBSOIL & SUBGRADE CONDITIONS. BASED ON THESE BORINGS AND PAST CONSTRUCTION PROJECTS THERE EXISTS A POTENTIAL THAT SOME OF THE SILTY SANDY MATERIALS MAY BE SOFT AND SATURATED. IF THE SUBGRADE BECOMES UNSTABLE THE RESIDENT PROJECT REPRESENTATIVE WILL DETERMINE THE TREATMENT TO BE USED TO STABILIZE THE SUBGRADE AFTER THE EXISTING PULVERIZED MATERIALS HAVE BEEN REMOVED FROM THE SECTION. THE SUBGRADE AND SUBSOIL TREATMENTS THAT WILL BE USED WILL BE ONE OF THE FOLLOWING:

- SUBGRADE & SUBSOILS STABLE - SCARIFY AND RECOMPACT 6 INCHES OF SUBGRADE TO 95% RELATIVE COMPACTION.
- SUBGRADE AND SUBSOILS UNSTABLE - EXCAVATE UNSTABLE SOILS TO DEPTHS OF 1 TO 3 FEET, AERATE AND DRY EXCAVATED SOILS, REPLACE DRIED SOILS AND COMPACT TO 95% RELATIVE COMPACTION. PAYMENT WILL BE MADE UNDER UNCLASSIFIED EXCAVATION FOR THIS TREATMENT, IF NEEDED).
- SUBGRADE AND SUBSOILS UNSTABLE - EXCAVATE UNSTABLE SOILS UP TO A DEPTH OF 2 FEET, PLACE TENSAR INTERAX NX650 GEOGRID (OR APPROVED EQUAL), HAUL, PLACE, AND COMPACT FOR ASPHALT AGGREGATE SUBBASE OR IMPORTED AGGREGATE SUBBASE TO BRING OVEREXCAVATION BACK TO SUBGRADE ELEVATION, AND COMPACT TO 95% RELATIVE COMPACTION. PAYMENT WILL BE MADE UNDER THE MUCK EXCAVATION(P-152) ITEM AND FOR ASPHALT AGGREGATE SUBBASE(P-207) OR SUBBASE COURSE(P-154) AND GEOGRID(P-154).
- UNSTABLE SUBGRADE - STOP ALL WORK IN UNSTABLE AREA. ROLL SUBGRADE WITH SCRAPERS OR ROLLERS, AS DIRECTED, UNTIL SECTION WEAVES UNDER LOAD. AT THAT TIME CEASE ALL WORK IN THE AREA FOR 24 HOURS, THEN REPEAT UNTIL SUBGRADE IS STABLE UNDER ALL LOADS. PAYMENT FOR THIS WORK SHALL BE MADE ON A TIME AND MATERIALS BASIS BASED ONLY ON EQUIPMENT AND MATERIALS USED. THERE WILL BE NO PAYMENT FOR TIME LOST OR OTHER DELAYS.
- AN APPROPRIATE TIME EXTENSION WILL BE MADE FOR ANY EXTRA WORK.

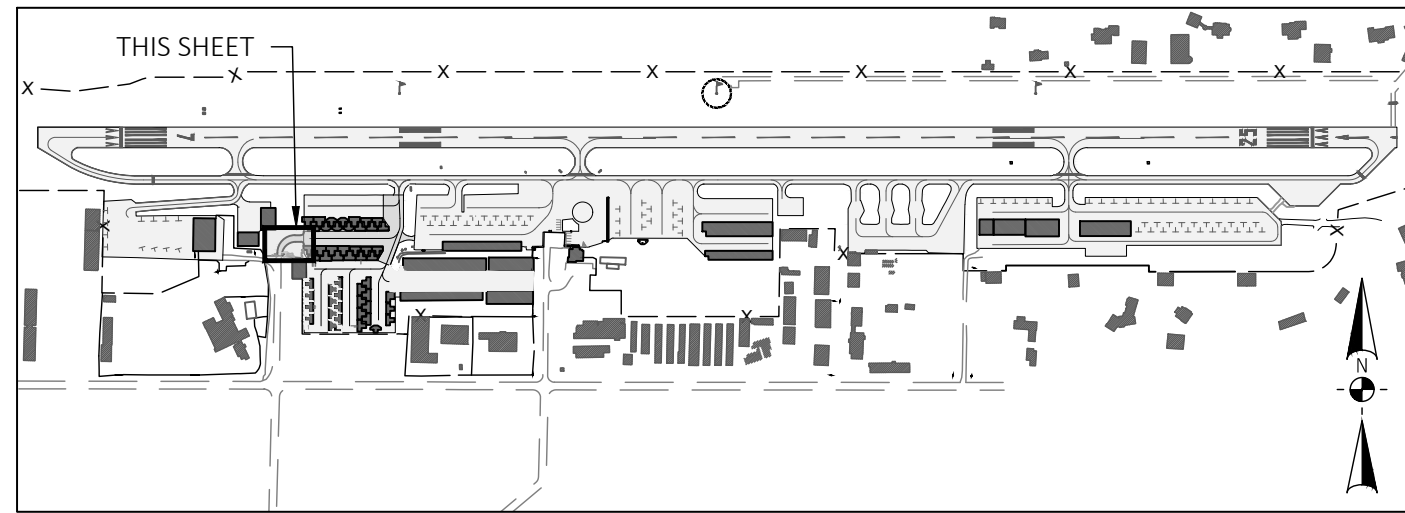
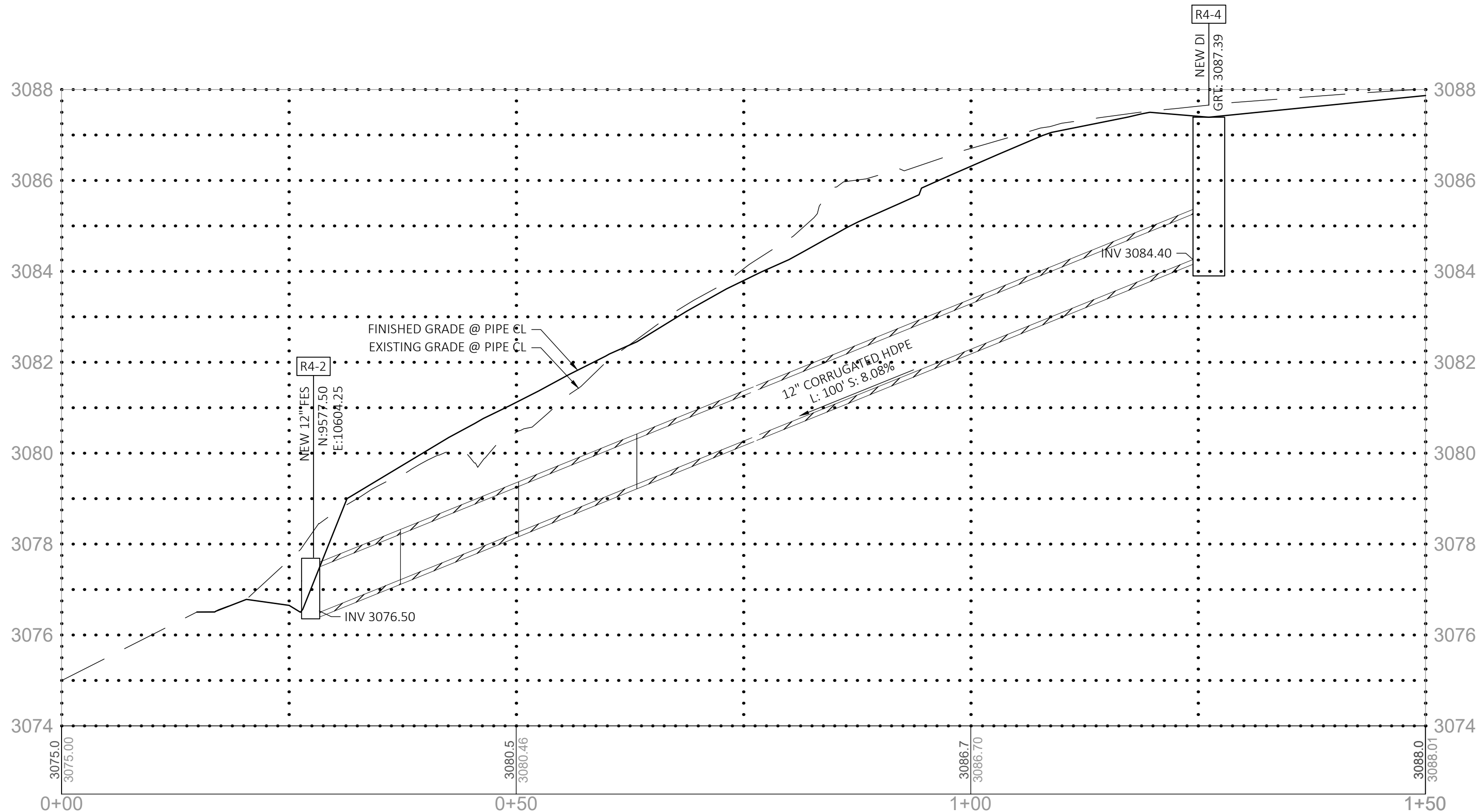
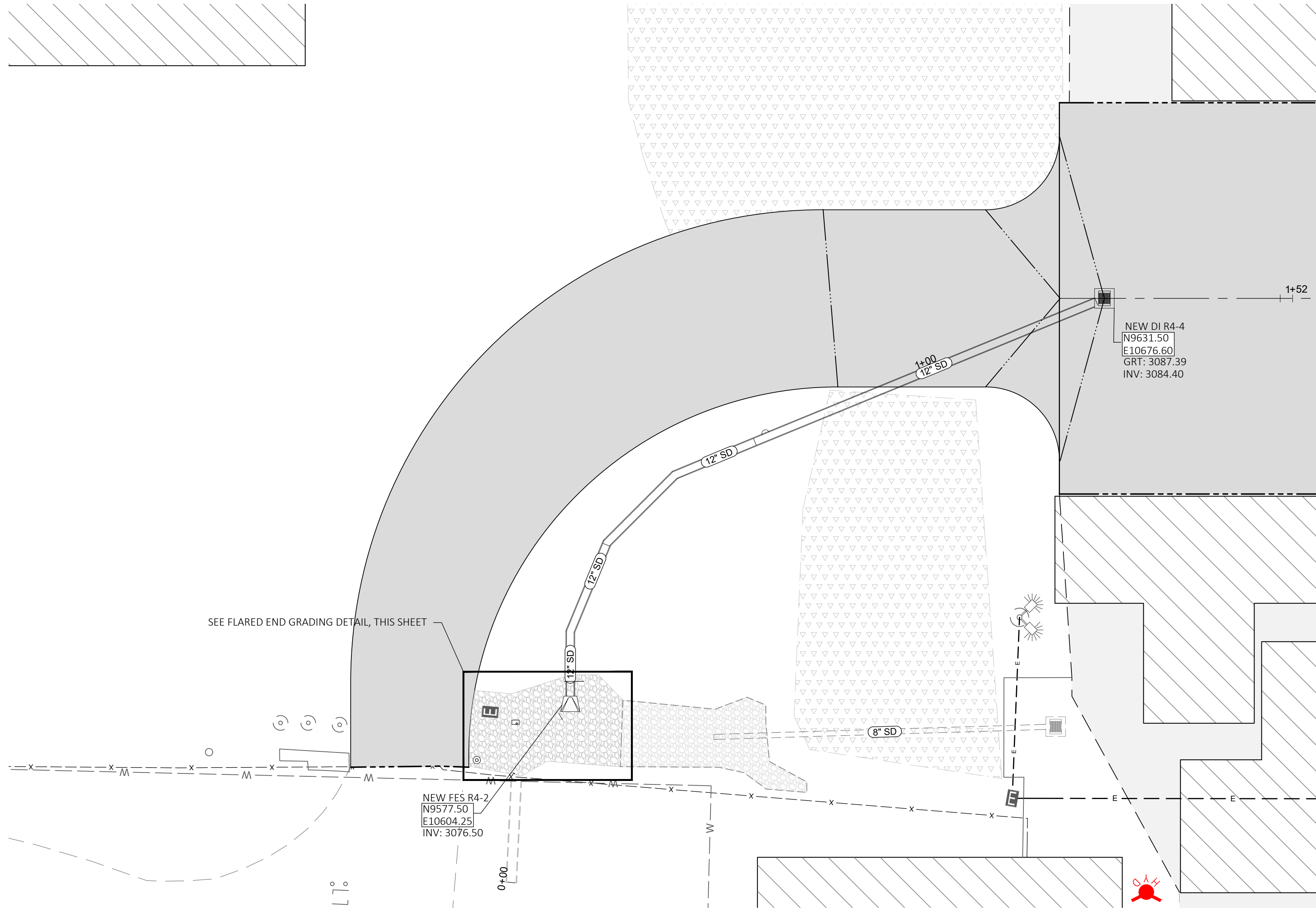
SUBGRADE STABILIZATION SECTION



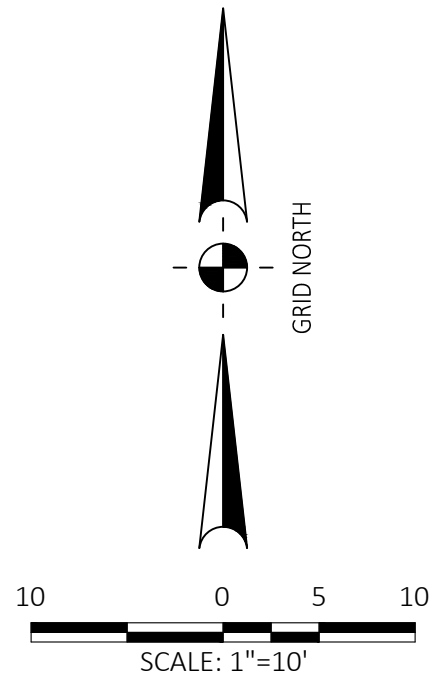
ENGINEER OF RECORD	BY	DATE	REVISIONS	No.



G:\38 NEVADA COUNTY\07 RAMP\A1\07 DB STORM.DWG PLOTTED BY David Baltazar 3/3/2025 11:35 AM



LOCATION MAP

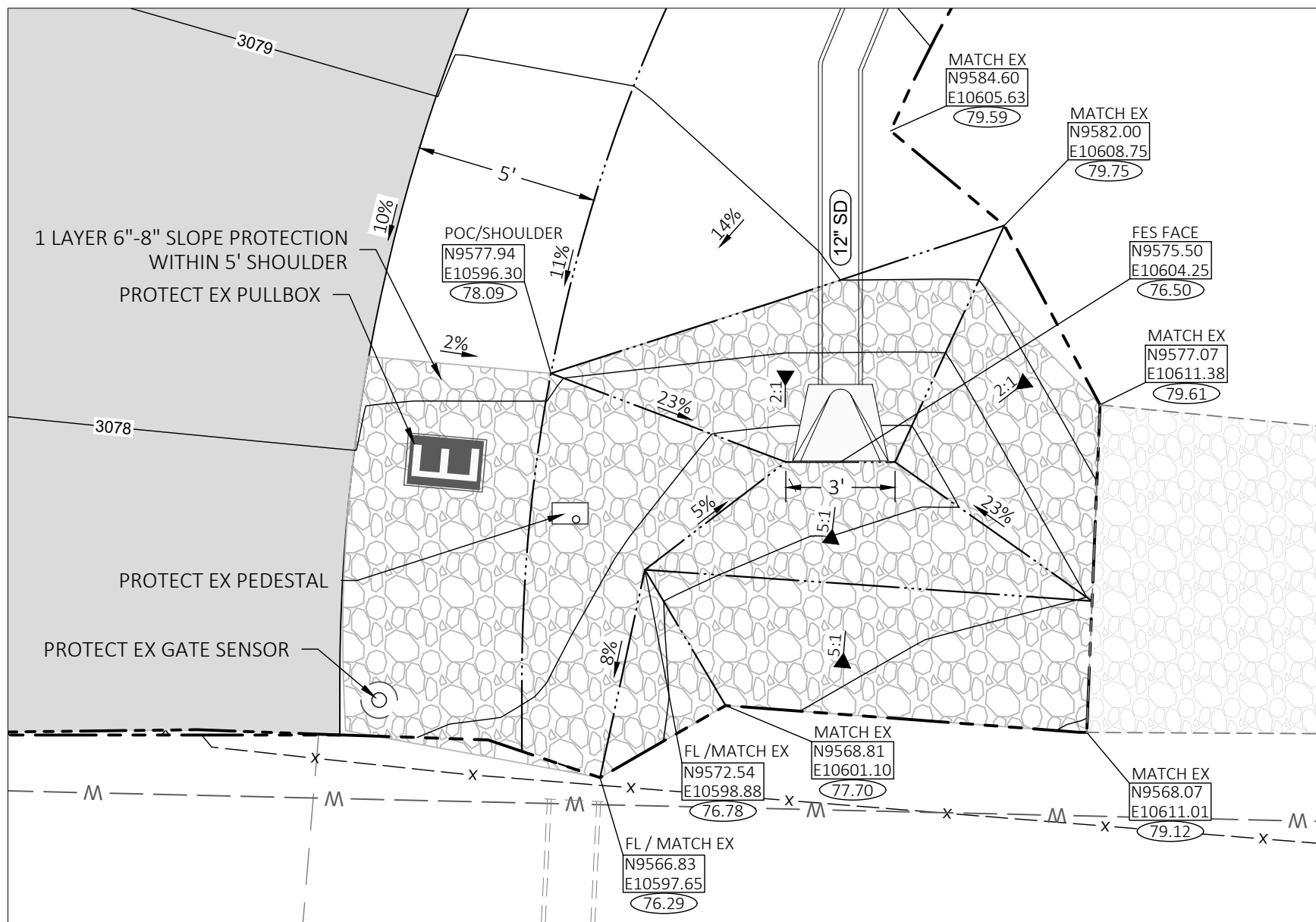


LEGEND

- N9738.46  
E13727.35  
22.00  
22.00
- EXISTING GRADE ELEVATION
- EXISTING GRADE ELEVATION
- EXISTING PAVEMENT
- NEW PAVEMENT SECTION
- EXISTING BUILDING
- CENTERLINE
- GRADE BREAK
- 3090
- 3090
- EXISTING GRADE CONTOUR
- EXISTING GRADE CONTOUR
- MATCH EXISTING GRADE
- EXISTING FENCE
- E
- W
- EXISTING ELECTRICAL DUCT
- EXISTING WATER LINE
- 12" SD
- 12" SD
- EXISTING STORM DRAIN
- EXISTING DROP INLET

NOTES:

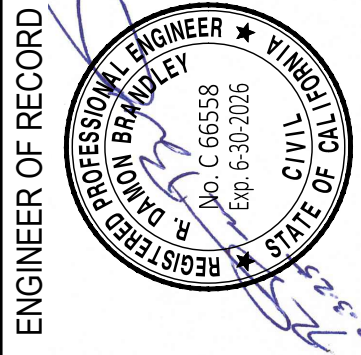
- CONTRACTOR SHALL CONSTRUCT CURVED DRAINLINE SEGMENT WITHIN THE DISTURBED LIMITS OF THE ACCESS ROAD SHOULDER, MIN OFFSET FROM EDGE OF PAVEMENT SHALL BE 5'. AT THEIR DISCRETION, CONTRACTOR MAY UTILIZE CURVED PIPE, OR BEND PIPE AND JOINTS WITHIN MANUFACTURER'S SPECIFIED LIMITS, OR UTILIZE STRAIGHT SEGMENTS AND MANUFACTURED BENDS. ALL JOINTS SHALL BE FULLY SECURE AND WATER TIGHT. PIPE ALIGNMENT SHOWN ON THIS DRAWING WAS ACHIEVED WITH STANDARD 13' STRAIGHT SEGMENTS AND 22.5° BEND FITTINGS.
- NEW DRAINLINE, DROP INLET, FLARED END SECTION, AND SLOPE PROTECTION WILL BE CONSTRUCTED AS PART OF SCHEDULE A. IF ALTERNATE A1 IS NOT SELECTED, EXISTING ACCESS ROAD WILL BE SAW CUT AND PATCHED FOR NEW PIPE INSTALLATION.
- SEE SHEET 11 FOR STORM DRAIN DETAILS.



FLARED END GRADING & SLOPE PROTECTION

SCALE: 1" = 4'

SCHEDULE A  
ALTERNATE A1



ENGINEER OF RECORD

REVISIONS	BY	DATE
No.		

NEVADA COUNTY AIRPORT  
CALIFORNIA  
RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD  
STORM DRAIN PLAN & PROFILE

DATE	2/7/2025
DRAWN	KDC
CHECKED	DB
PROJECT No.	36.07
FILE	3607.09.Storm
SCALE	1"=10'
SHEET No.	10 of 17

**BRANDLEY**  
**ENGINEERING**

6125 KING ROAD, SUITE 201 · LOOMIS, CALIFORNIA 95650 · (916) 652-4725







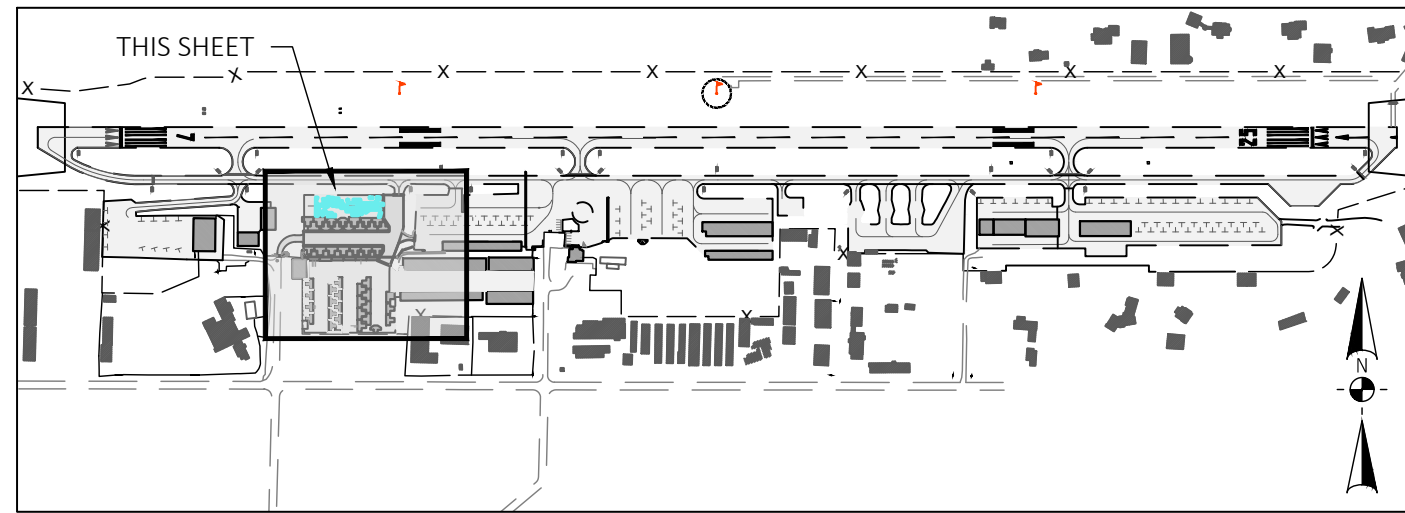
G:\36 NEVADA COUNTY\07 RAMP\BID SET\3607.12 CRACKS.DWG PLOTTED BY David Baltazar 3/3/2025 11:37 AM

GENERAL CRACK SEAL NOTES:

1. CONTRACTOR SHALL GIVE 2 WEEKS NOTICE TO THE AIRPORT PRIOR TO BEGINNING ANY WORK AND CLOSING ANY WORK AREAS.
2. CONTRACTOR SHALL PROTECT ALL RUNWAY AND TAXIWAY LIGHTING, NAVAIDS AND SURVEY MONUMENTS FROM DAMAGE OR DEBRIS.
3. ALL RUNWAYS AND TAXIWAYS SHALL BE CLEAN OF ALL DEBRIS PRIOR TO OPENING FOR AIRCRAFT OPERATIONS.
4. LIMITS OF ALL CRACK SEAL SHALL BE MARKED ON PAVEMENTS BY RESIDENT PROJECT REPRESENTATIVE (RPR) PRIOR TO ANY WORK IN THE AREA BY THE CONTRACTOR.
5. RPR SHALL DESIGNATE IN THE FIELD WHICH CRACKS ARE TO BE SEALED AS TYPE C OR TYPE S.
6. ALL CRACKS TO BE SEALED MUST BE CLEANED OF ALL DEBRIS AND VEGETATION PRIOR TO REPAIR AND SEALED PER DETAILS SHOWN ON SHEET 13.
7. TYPE C CRACKS INCLUDE THE SEALING OF NEW CRACKS THAT DO NOT HAVE EXISTING SEALANT IN THEM AND ALSO THE RESEALING OF EXISTING CRACKS THAT HAVE OLD SEALANT IN THEM. SEE DETAILS ON SHEET 13 FOR HOW TO TREAT EACH OF THESE CRACKS. TYPE C CRACKS ARE ALL PAID UNDER A SINGLE UNIT PRICE BID ITEM. IT IS ESTIMATED THAT THERE ARE 7,500 LINEAR FEET OF NEW CRACKS AND 700 LINEAR FEET OF EXISTING CRACKS WITHIN THIS BID ITEM. THIS INFORMATION IS AN ESTIMATE TO ASSIST BIDDERS DURING BIDDING. ALL BIDDERS ARE INVITED TO INSPECT THE SITE PRIOR TO THE BID TO PROPERLY UNDERSTAND THE CONDITION OF EACH CRACK. FINAL QUANTITY OF ALL CRACKS SEALED, TYPE C OR S WILL BE MEASURED DURING CONSTRUCTION FOR FINAL PAY QUANTITIES.
8. CONTRACTOR SHALL FINISH ALL TOUCH UP, FILL SINK HOLES OF ALL SEALED CRACKS PRIOR TO COMPLETION OF EACH PHASE.
9. CRACKS SHOWN ON PLAN WERE IDENTIFIED BASED ON AERIAL PHOTOGRAPHS FROM DRONE OPERATION. ALL CRACKS TO BE SEALED WILL BE IDENTIFIED BY RPR PRIOR TO SEALING.
10. SEE SHEETS 2 FOR PHASING PLANS AND NOTES.
11. SEE SHEET 5 FOR MARKING REMOVAL LIMITS. ALL MARKING REMOVAL SHALL BE PERFORMED PRIOR TO CRACK SEALING OPERATIONS.
12. SEE SHEETS 14 TO 15 FOR MARKING PLANS AND DETAILS. NEW AIRFIELD MARKING TO BE INSTALLED AFTER CRACK SEALANT HAS BEEN INSTALLED AND ADEQUATELY CURED.

LEGEND

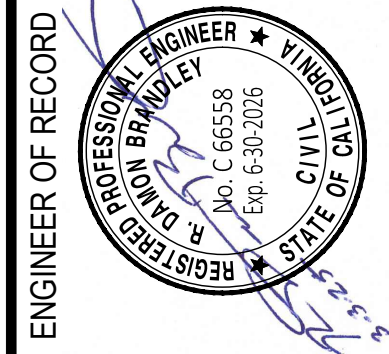
- EXISTING PAVEMENT
- EXISTING BUILDING
- TYPE C - RESEAL EXISTING CRACK
- TYPE C - SEAL NEW CRACK
- TYPE S - CRACK SEAL / SEAL SURFACE



LOCATION MAP



SCHEDULE B  
ALTERNATE B1



**BRANDLEY**  
ENGINEERING

6125 KING ROAD, SUITE 201 · LOOMIS, CALIFORNIA 95650 · (916) 652-4725

NEVADA COUNTY AIRPORT  
CALIFORNIA  
NEVADA COUNTY  
**RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD**  
CRACK SEAL PLAN

DATE	2/7/2025
DRAWN	KDC
CHECKED	DB
PROJECT No.	36.07
FILE	3607.12.Cracks
SCALE	1"=30'
SHEET No.	12 of 17





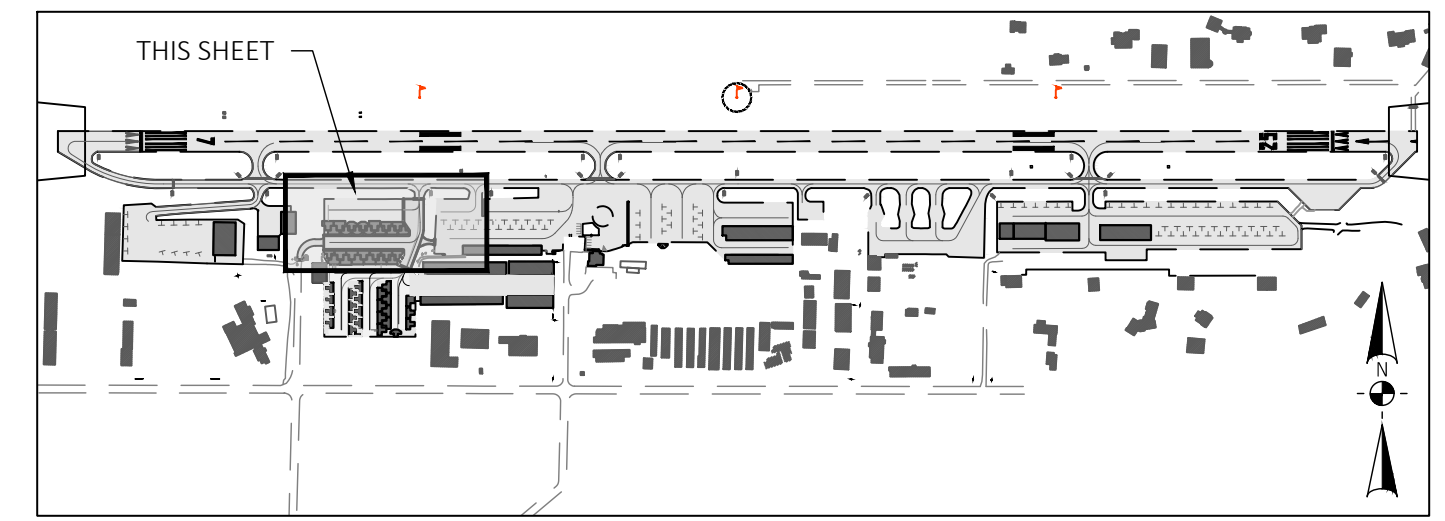


## NOTES:

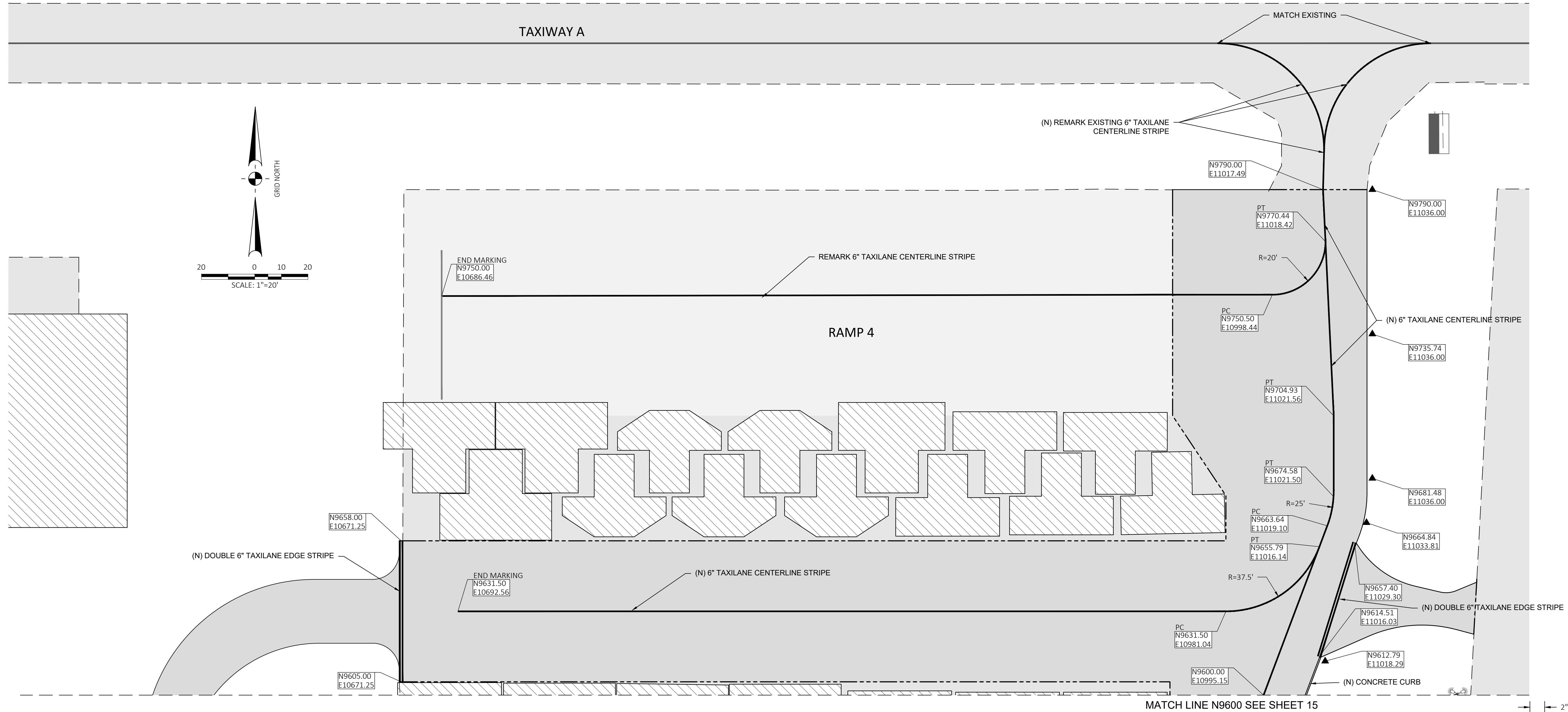
- EXISTING AND NEW MARKING LAYOUT SHALL BE VERIFIED BY CONTRACTOR PRIOR TO PAINTING.
- ANY SECTION OF A MARKING THAT IS NOT LOCATED AS SPECIFIED SHALL BE REMOVED AND THE NEW MARKING AND/OR REMARKING SHALL BE LOCATED AS SPECIFIED USING TWO COATS OF PAINT AND REFLECTIVE BEADS.
- SEE SHEET 4 FOR BID SCHEDULE KEY MAP
- FOR MARKING REMOVAL SEE SHEET 5.
- ALL MARKING REMOVAL SHALL BE DONE SUCH AS TO MINIMIZE DAMAGE TO EXISTING PAVEMENTS. PLACE SS1H TACK COAT ON ALL MARKING REMOVAL AREAS AFTER MARKING REMOVAL IS COMPLETE. SS1H TACK COAT COSTS ARE INCIDENTAL TO MARKING REMOVAL BID ITEMS

## LEGEND

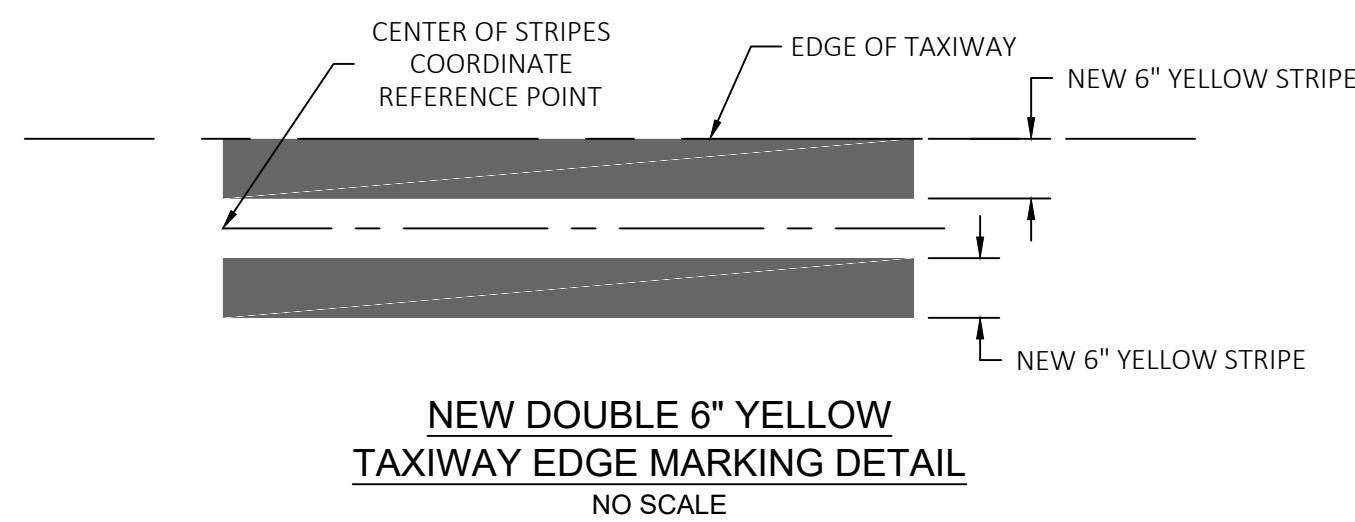
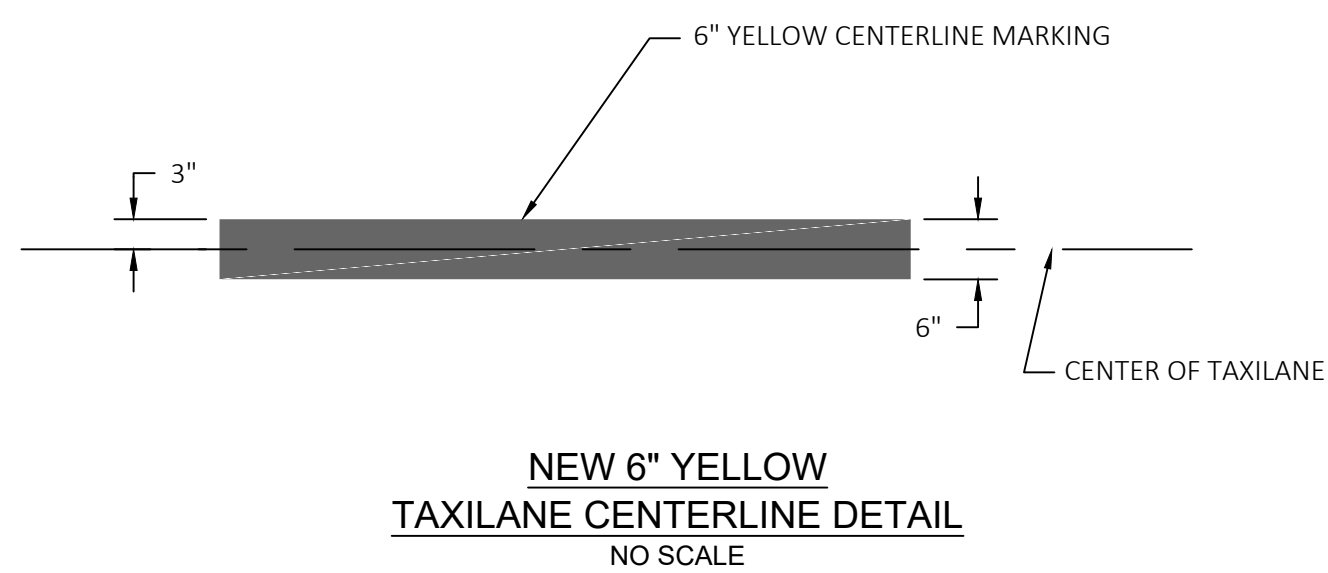
- EXISTING EDGE OF PAVEMENT
- EXISTING MARKING
- NEW MARKING
- NEW REFLECTIVE TAXIWAY EDGE MARKER



LOCATION MAP

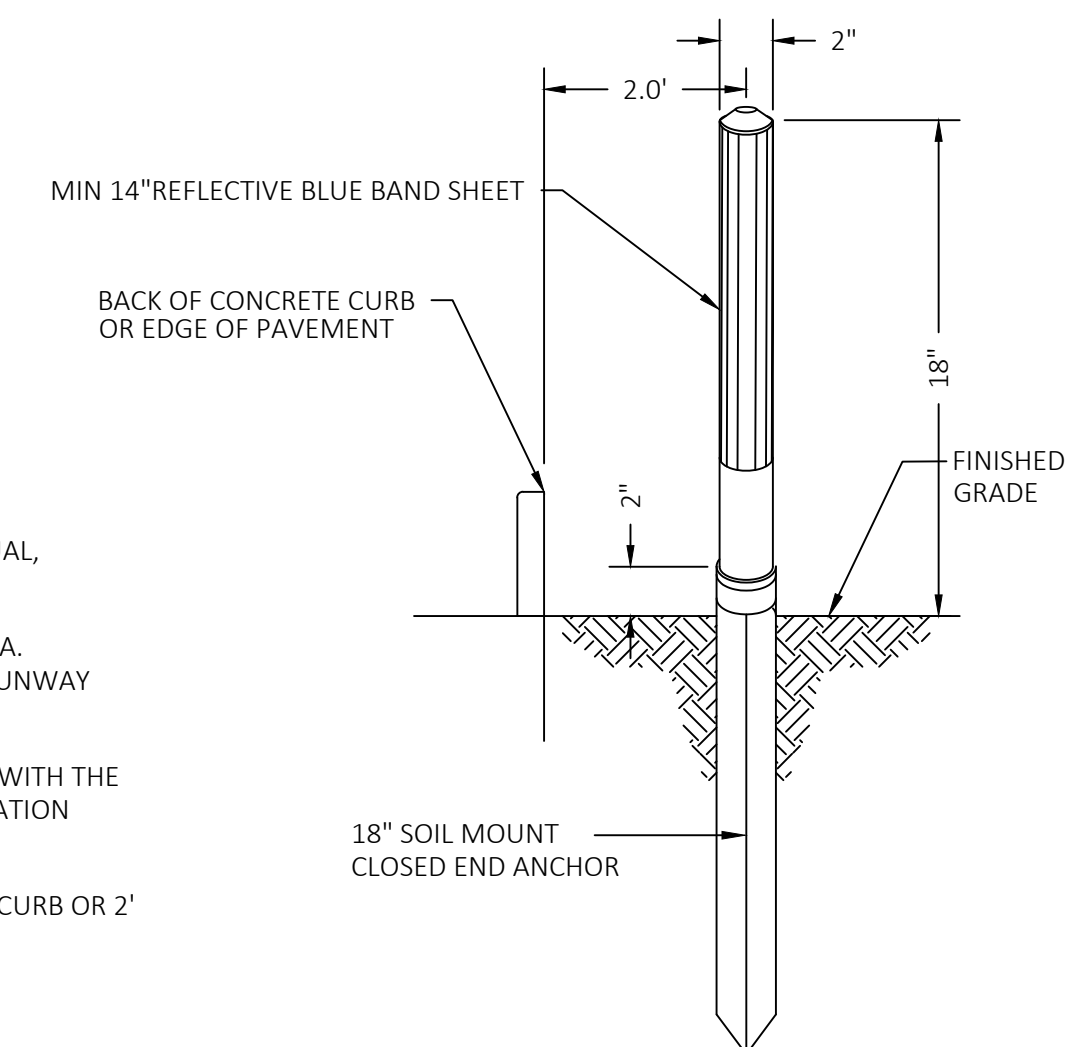


MATCH LINE N9600 SEE SHEET 15



## REFLECTIVE MARKER NOTES:

- MARKERS SHALL BE MADE OF POLYETHYLENE OR APPROVED EQUAL, FLEXIBLE, AND APPROVED BY THE FAA.
- MARKERS SHALL BE MANUFACTURED IN ACCORDANCE WITH F.A.A. ADVISORY CIRCULAR 150/5345-39D, SPECIFICATIONS FOR L-853, RUNWAY AND TAXIWAY RETROREFLECTIVE MARKERS.
- CONTRACTOR SHALL FURNISH AND INSTALL MARKER COMPLETE WITH THE GROUND ANCHOR, TUBE AND COLOR BAND PLACED AT THE LOCATION SHOWN ON MARKING PLAN.
- LOCATE REFLECTIVE MARKER 2' BEHIND BACK FACE OF CONCRETE CURB OR 2' FROM EDGE OF PAVEMENT IN AREAS WITHOUT CURBING.

ELEVATED TAXIWAY REFLECTIVE MARKER DETAIL  
NO SCALESCHEDULE A  
SCHEDULE B

ENGINEER OF RECORD

BY DATE

REVISIONS

No.

NEVADA COUNTY AIRPORT

CALIFORNIA

RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD

MARKING PLAN - SHEET 1 AND DETAILS

DATE	2/7/2025
DRAWN	TS
CHECKED	DB
PROJECT No.	36.07
FILE	3607.14.Marking
SCALE	1"=20'

SHEET No.  
14 OF 17BRANDLEY  
ENGINEERING

6125 KING ROAD, SUITE 201 · LOOMIS, CALIFORNIA 95650 · (916) 652-4725

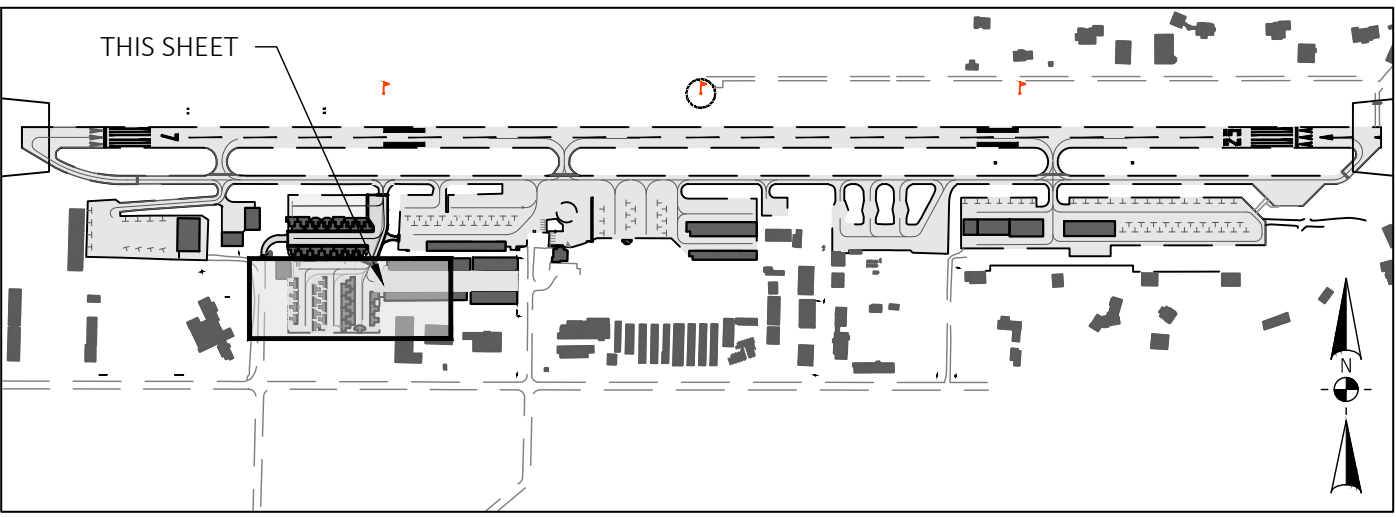
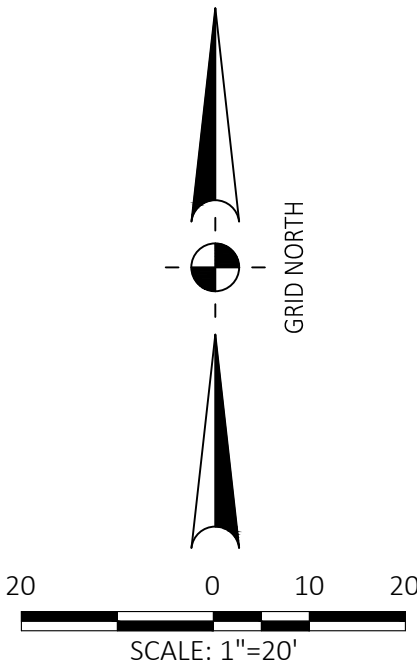


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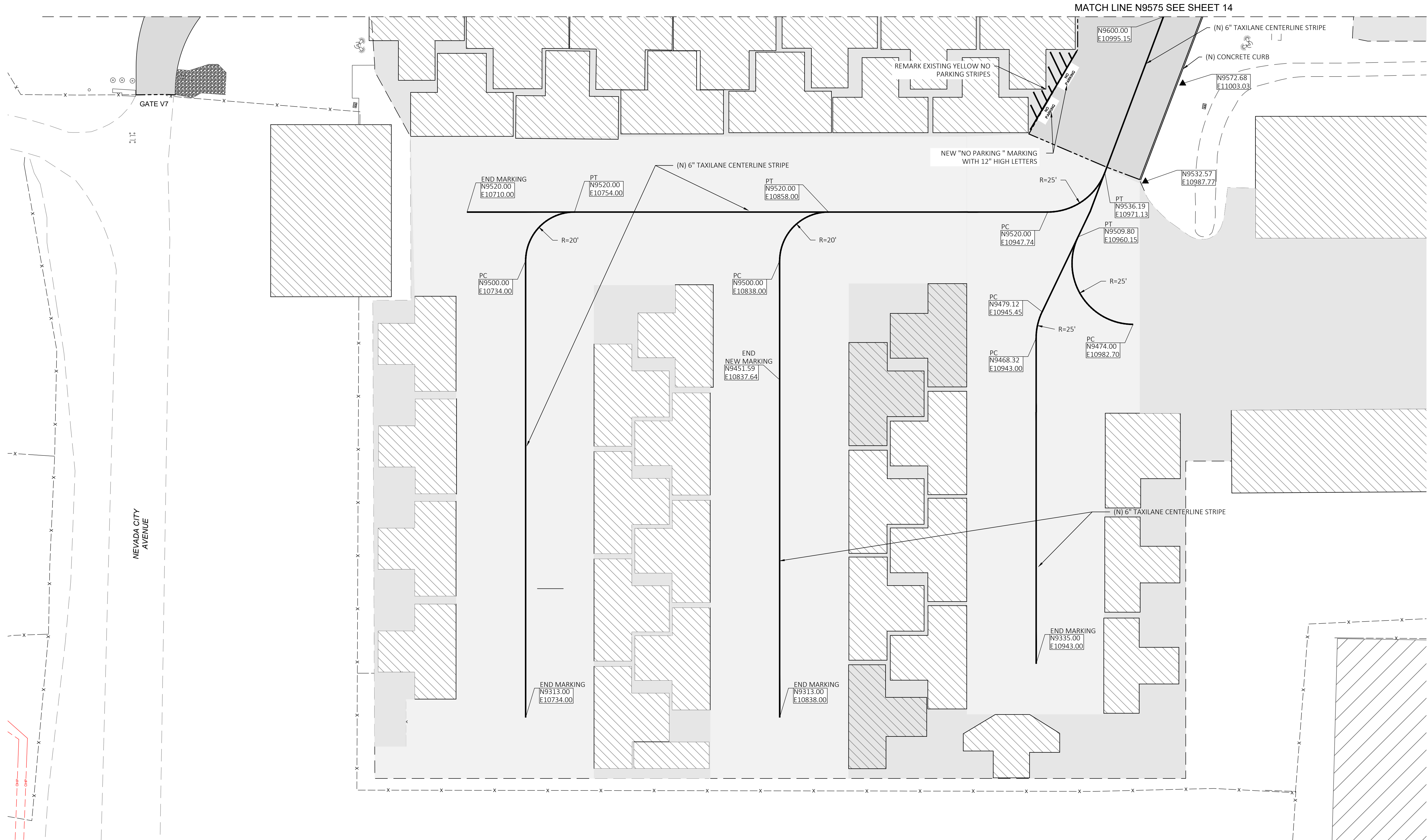
- EXISTING AND NEW MARKING LAYOUT SHALL BE VERIFIED BY CONTRACTOR PRIOR TO PAINTING.
- ANY SECTION OF A MARKING THAT IS NOT LOCATED AS SPECIFIED SHALL BE REMOVED AND THE NEW MARKING AND/OR REMARKING SHALL BE LOCATED AS SPECIFIED USING TWO COATS OF PAINT AND REFLECTIVE BEADS.
- SEE SHEET 4 FOR BID SCHEDULE KEY MAP
- FOR MARKING REMOVAL SEE SHEET 5.
- ALL MARKING REMOVAL SHALL BE DONE SUCH AS TO MINIMIZE DAMAGE TO EXISTING PAVEMENTS. PLACE SS1H TACK COAT ON ALL MARKING REMOVAL AREAS AFTER MARKING REMOVAL IS COMPLETE. SS1H TACK COAT COSTS ARE INCIDENTAL TO MARKING REMOVAL BID ITEMS

LEGEND

- EXISTING EDGE OF PAVEMENT
- EXISTING MARKING
- NEW MARKING
- NEW REFLECTIVE TAXIWAY EDGE MARKER

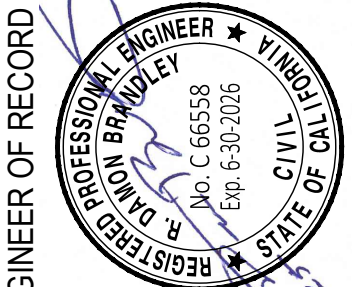


LOCATION MAP



NEVADA COUNTY  
CALIFORNIA  
**RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD**

MARKING PLAN - SHEET 2



ENGINEER OF RECORD

No.	REVISIONS	BY	DATE

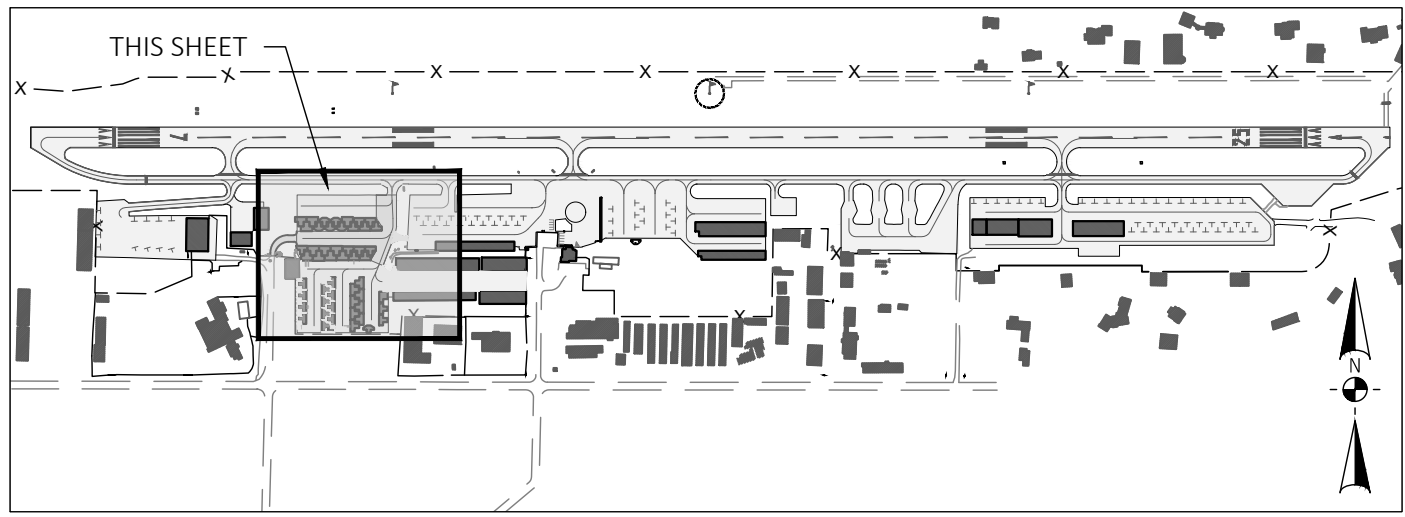
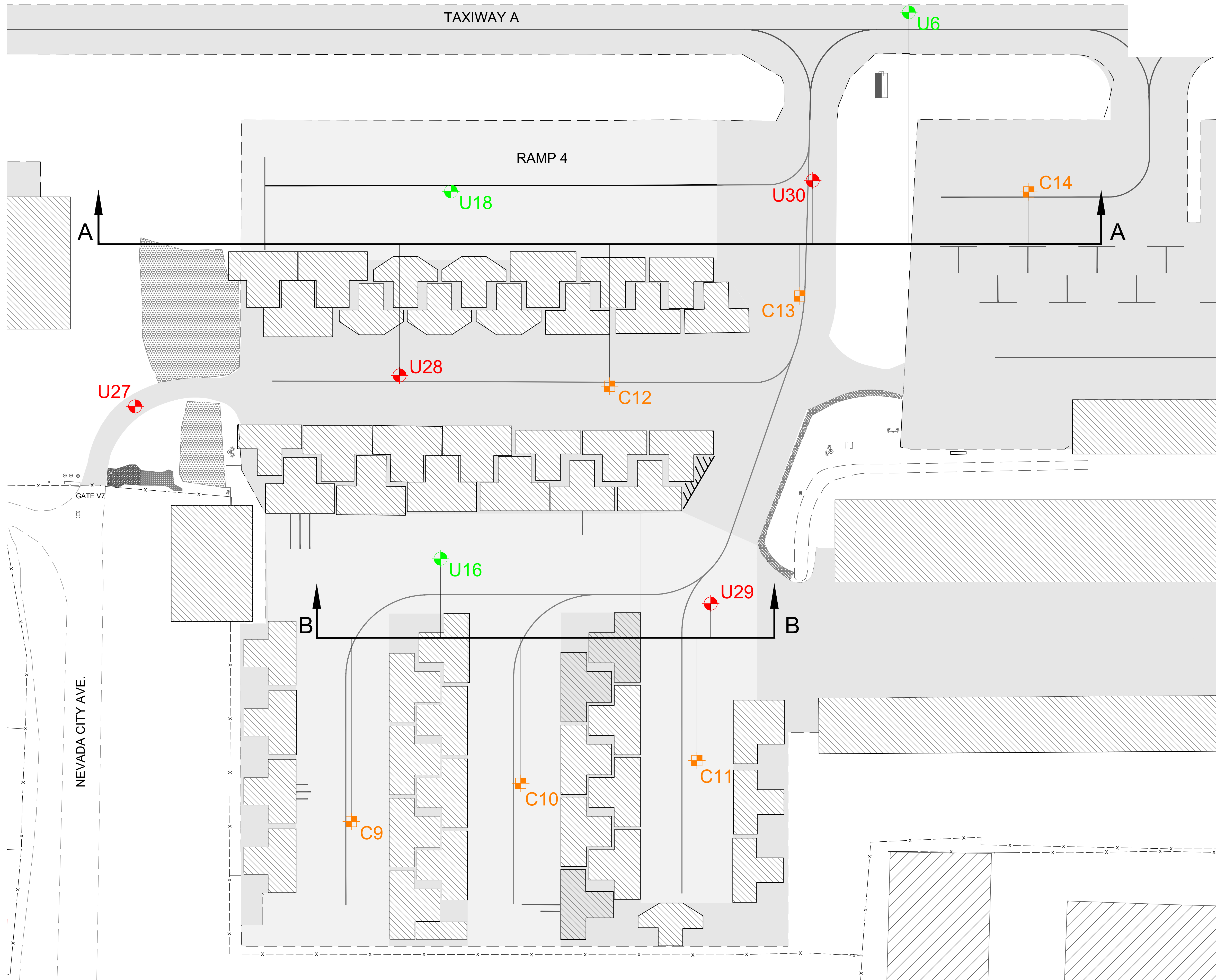
DATE	2/7/2025
DRAWN	TS
CHECKED	DB
PROJECT No.	36.07
FILE	3607.14.Marking
SCALE	1"=20'

SHEET No.  
15 OF 17

SCHEDULE A  
SCHEDULE B

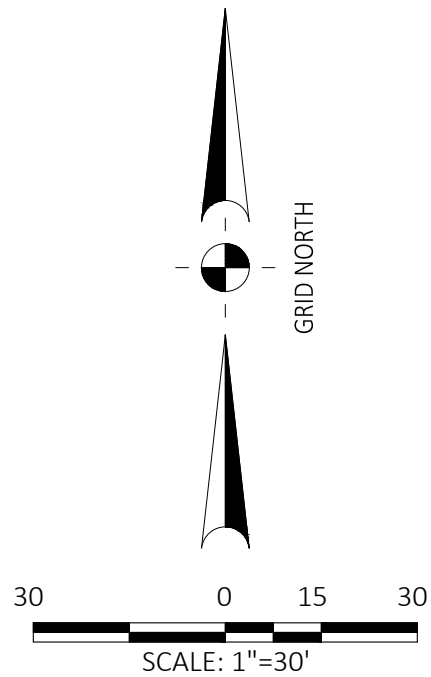


G:\36 NEVADA COUNTY\07 RAMP\416D SET\3607.16.SOLID.DWG PLOTTED BY David Baltazar 3/3/2025 11:37 AM



LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDING
- EXISTING FENCE
- U18 SOIL BORING LOCATION - 2020
- U30 SOIL BORING LOCATION - 2024
- C14 PAVEMENT CORE LOCATION - 2020



SCHEDULE A  
SCHEDULE B

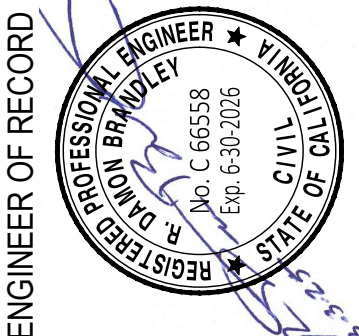
NEVADA COUNTY  
NEVADA COUNTY  
CALIFORNIA

**RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD**

SOIL BORING LAYOUT

DATE 2/7/2025  
DRAWN DMB  
CHECKED DB  
PROJECT No. 36.07  
FILE 3607.16.Soil  
SCALE 1"=30'

SHEET No.  
16 of 17



No.	REVISIONS	BY	APR	DATE

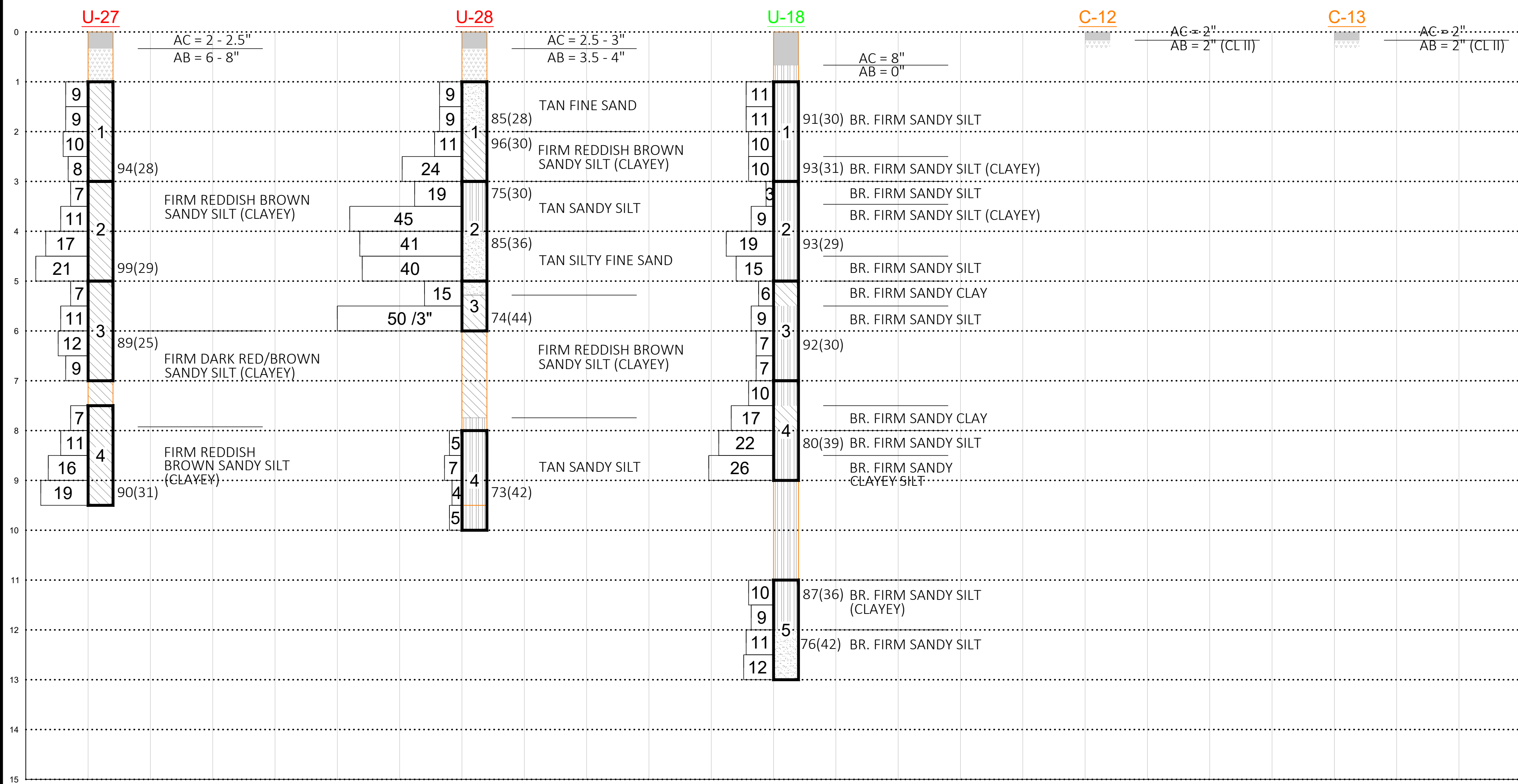
ENGINEER OF RECORD

**BRANDLEY**  
**ENGINEERING**

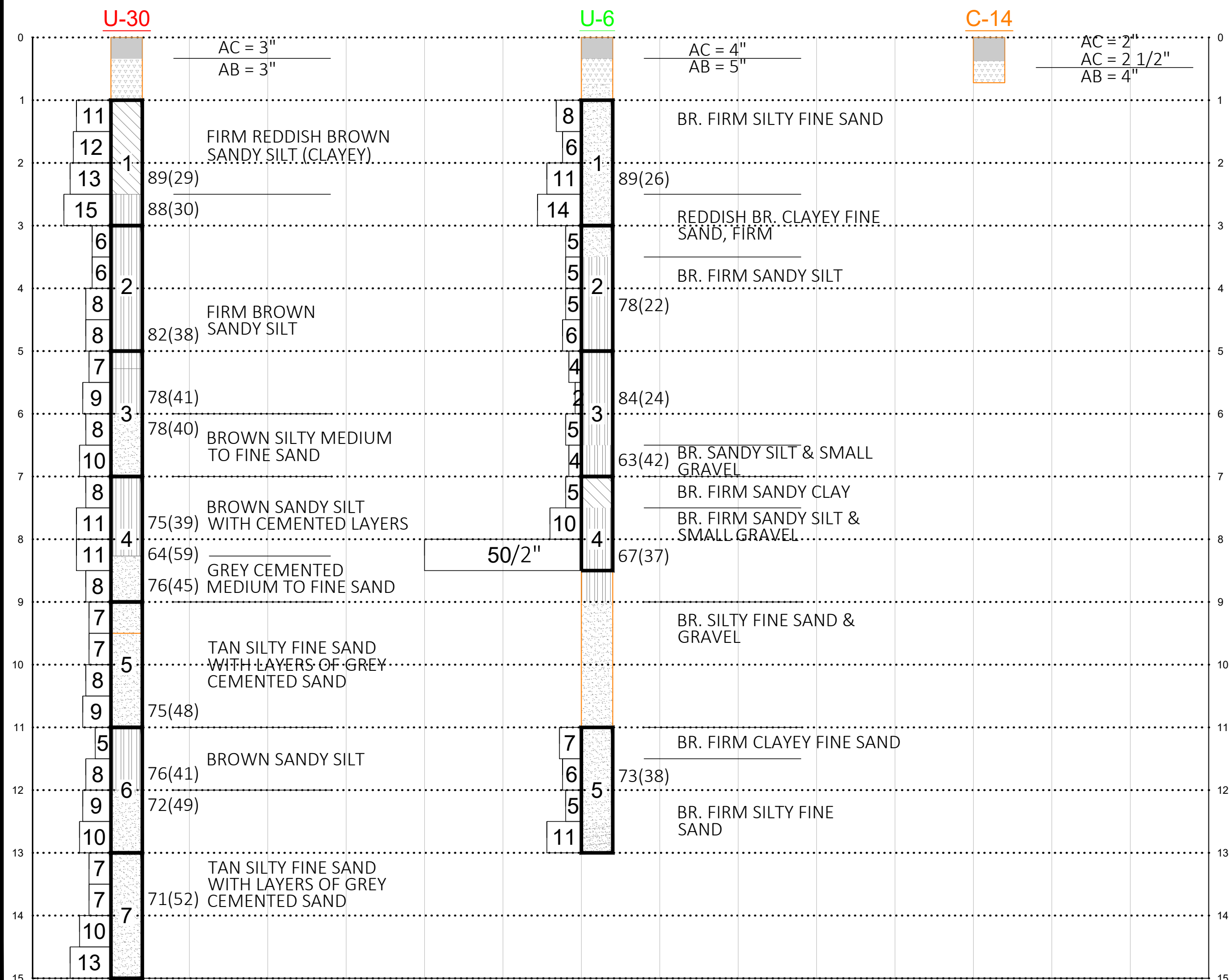
6125 KING ROAD, SUITE 201 · LOOMIS, CALIFORNIA 95650 · (916) 652-4725



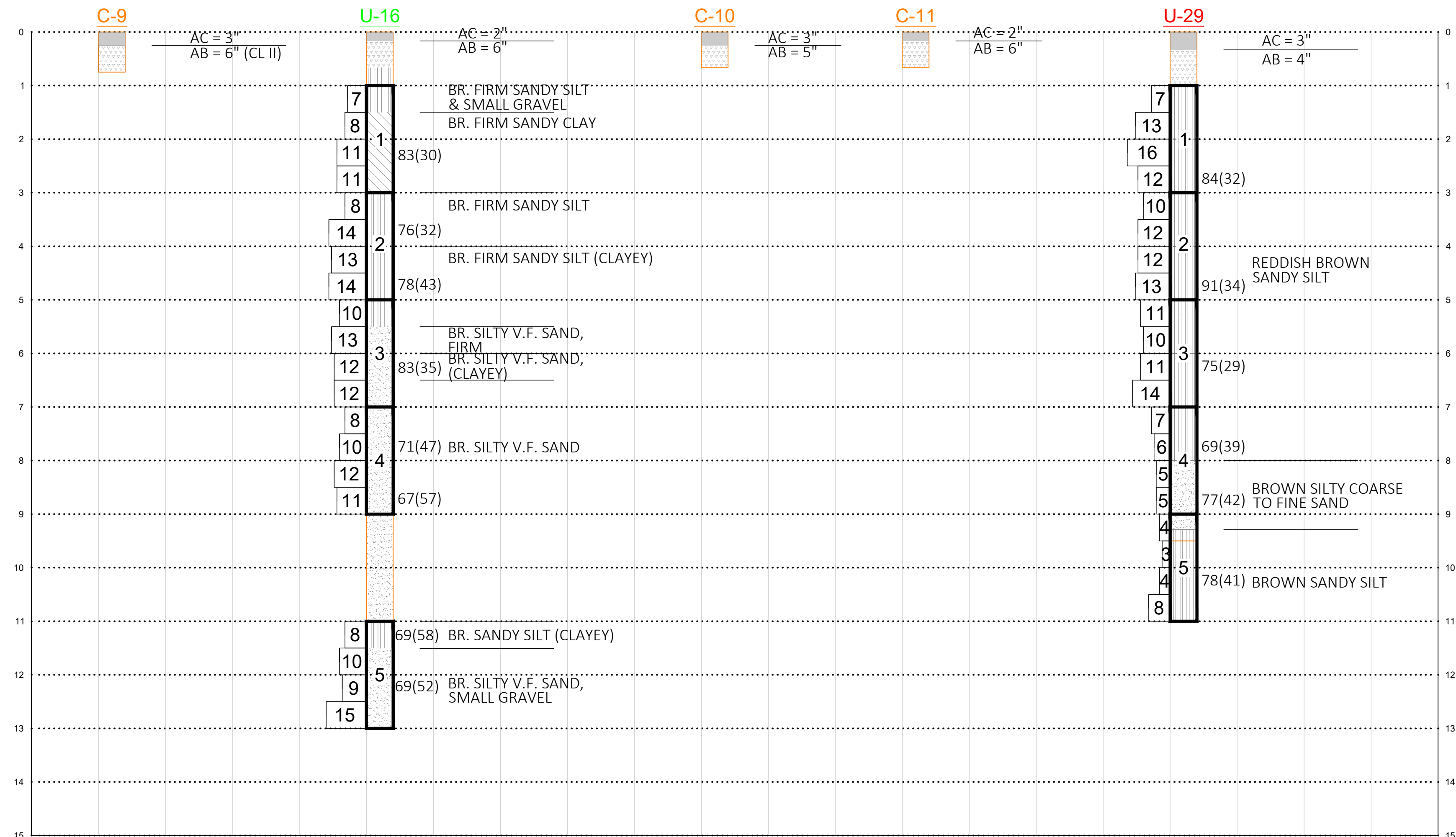
G:\38 NEVADA COUNTY\07 RAMP\A\BID SET\3607.16 SOIL.DWG PLOTTED BY David Baltazar 3/3/2025 11:37 AM



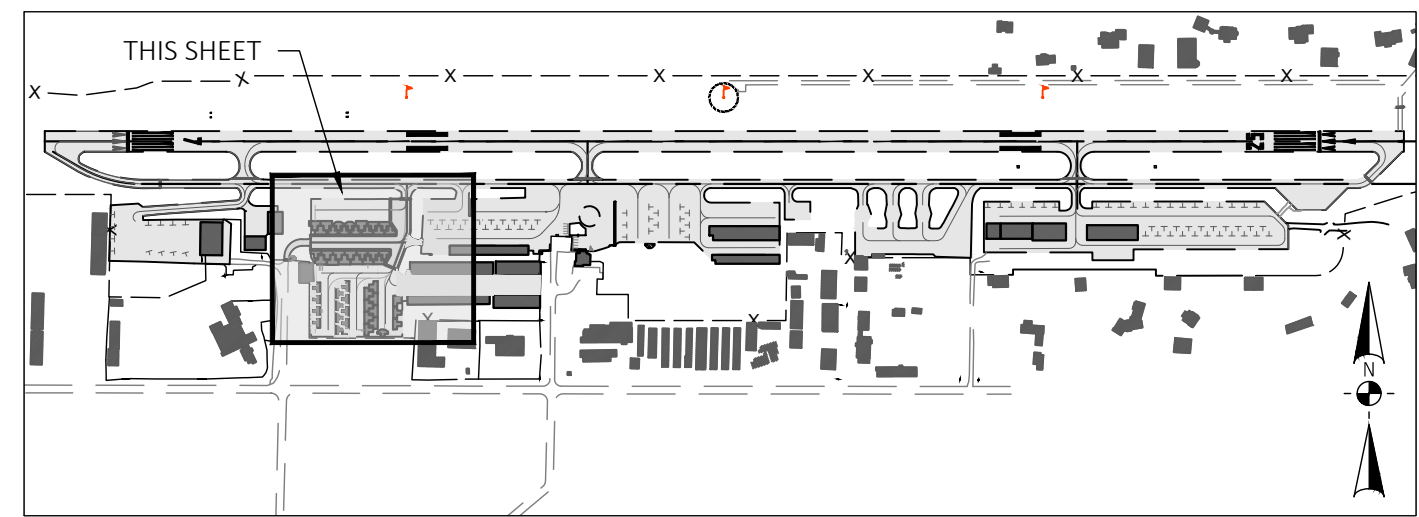
PROFILE A-A



PROFILE A-A

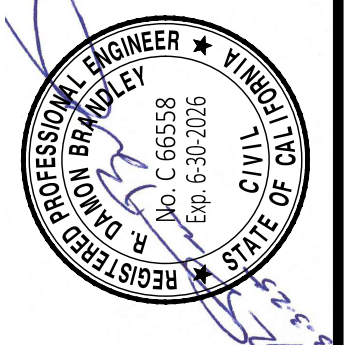
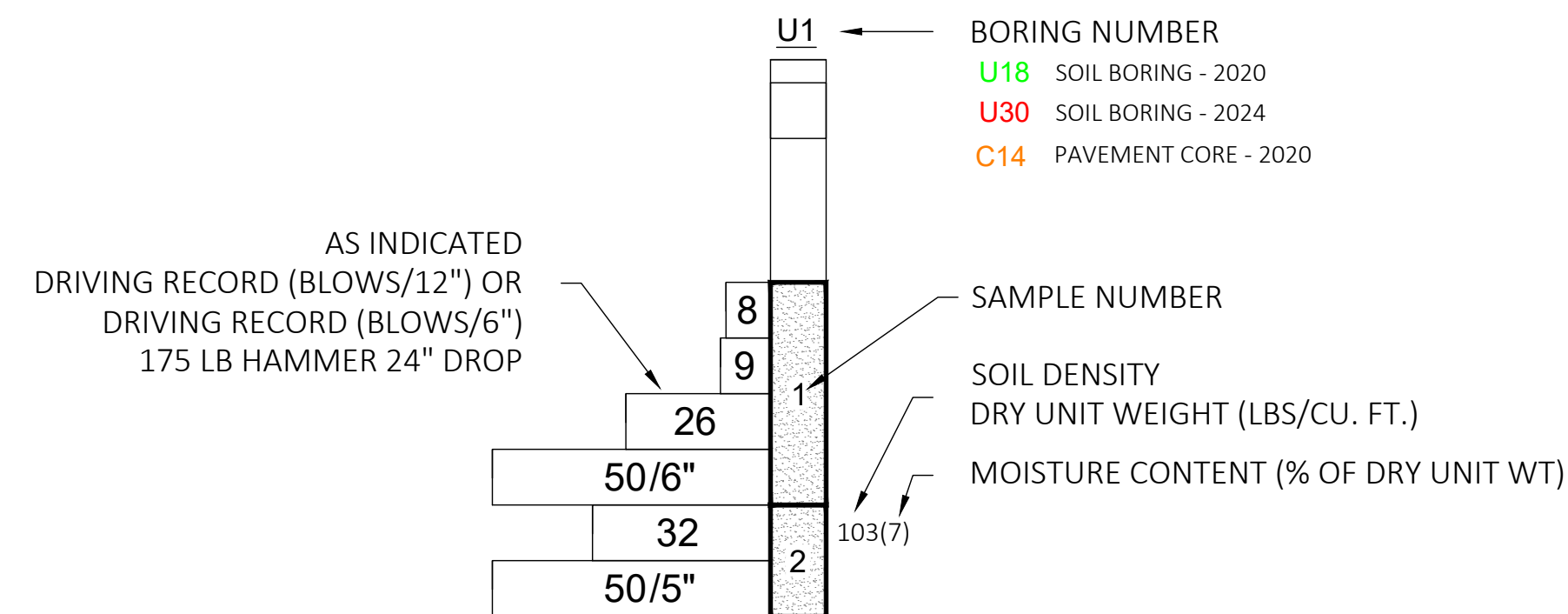


PROFILE B-B



LOCATION MAP

BORING LEGEND



REVISIONS	BY	DATE
No.		

NEVADA COUNTY AIRPORT  
RECONSTRUCT RAMP 4, CRACK SEAL RAMP 4  
TAXILANES & RECONSTRUCT RAMP 4 ACCESS ROAD

NEVADA COUNTY  
CALIFORNIA

SOIL PROFILES

DATE	2/7/2025
DRAWN	KDC
CHECKED	DB
PROJECT No.	36.07
FILE	3607.16 Soils
SCALE	1"=30'
SHEET No.	17 of 17

SCHEDULE A  
SCHEDULE B